



Town of Reading
16 Lowell Street
Reading, MA 01867

Andrew MacNichol
Staff Planner
Phone: 781.942-6670
Fax: 781.942-9071
Website: www.readingma.gov

September 12, 2022

Downtown Smart Growth District (DSGD) Plan Review
M.G.L. Chapter 40R
DECISION

Project: 459 Main Street
Applicant: GC Fodera Contracting, Inc.

To the Town Clerk:

This is to certify that, at a public hearing of the Community Planning and Development Commission opened on February 7, 2022, discussed on March 14, 2022, April 11, 2022, May 16, 2022, June 13, 2022, August 8, 2022, September 12, 2022 and closed on XXX by a motion duly made and seconded, it was voted:

“We, the Reading Community Planning and Development Commission, upon request from the GC Fodera Contracting, Inc., under Section 10.5 of the Zoning Bylaws of the Town of Reading, and MGL Chapter 40R, to consider the application for 40R Development Plan Review at 459 Main Street (Assessors Map 17, Lot 62) – do hereby vote XXX to **APPROVE** a 40R Development Plan subject to the Findings and Conditions below.”

Materials Submitted:

The following materials were submitted into the public record:

General Findings:

- 1) **Zoning:** The subject site (“Site”) is located within the underlying Business-B Zoning District and the Downtown Smart Growth District (DSGD) / 40R Overlay District. Properties located to the north, south, and west of the Site are also within the Business-B and DSGD Zoning Districts. The property abutting the Site to the east is split-zoned between the Business-B/DSGD Zoning District and the S-15 Residential Zoning District.

As defined within the Downtown Smart Growth District Design Guidelines, the Site is considered a Transitional Area because it directly abuts a lot containing a two-family dwelling.

→Therefore, Section 10 of the Design Guidelines applies to any 40R redevelopment of the Site.

- 2) **Overview & Site Context:** The Site totals 10,256 square feet in area and maintains a combined ~192.57 linear feet of frontage along Main Street to the west and Washington Street to the

south. The Site is abutted by: an existing 40R Mix-Use Development to the north; a two-family dwelling to the east; Washington Street to the south, and; Main Street to the west. A series of residential balconies and windows on the south façade of the abutting 4-story building known as Rise475 are located very close to the lot line with the Site.

The Site currently contains an ~1,800 square-foot, single-story commercial structure that operates as an automotive repair facility. It also maintains three curb cuts (one along Main Street and two along Washington Street) and an associated at-grade parking area.

The Applicant is seeking Development Plan approval from the CPDC under Section 10.5 of the Reading Zoning Bylaw and the Downtown Smart Growth District Design Standards & Guidelines, pursuant to M.G.L. Chapter 40R.

- 3) Wetlands / Floodplain: There are no wetland resource areas or buffers on or near the site, and the site is not within a 100-year floodplain.
- 4) Commercial Parking: The site is NOT within 300' of a municipal lot and is NOT exempt from providing off-street commercial parking. There is no on-street parking along the building's frontage.
- 5) Signage: No building signage is approved herein. Any future signage shall require the submittal of a Sign Permit Application and shall comply with Section 8.0 of the Zoning Bylaw and Section 9.0 of the Downtown Smart Growth District Design Guidelines.

Waivers pursuant to Section 10.5.12 and DSGD Design Standards & Guidelines:

Upon request of the Applicant, the Commission, in the interests of design flexibility and overall project quality, and upon a finding of consistency of such variation with the overall purpose and objectives of the DSGD and the Reading Master Plan, or if it finds that such waiver will allow the project to achieve the density, affordability, mix of uses and/or physical character allowed. The Commission shall take into consideration the following items when considering a waiver:

1. High performance energy efficient buildings and construction methods.
2. Projects with publicly accessible open space.
3. Projects that include retail and restaurants located on street level.
4. A demonstrated shared parking initiative that makes efficient use of land and existing parking supply.
5. The preservation or rehabilitation of historic properties or other buildings considered significant to the Town.

The CPDC has granted the following waiver from ZBL Section 10.5:

Density: The project may provide up to but no more than 5 residential units, which equates to a density of 21.24 units/acre where 20 units/acre is allowed by-right. A strict 20 unit/acre density would result in 4.7 units.

Conditions:

General:

- 1) **Public Health, Safety and Welfare:** If, at any time, the site becomes a nuisance to public health, safety or welfare (i.e., traffic spillover, excessive noise, unreasonable site illumination beyond the hours of operation, etc.) – as shall be evidenced by substantiated complaints to the Police Department or Public Services Office – the Applicant/Owner shall agree to work with staff to rectify the problem. Should the situation warrant it, an additional Site Plan Review by the CPDC may be required.

- 2) **Utilities:** All utilities, structures, frames and covers shall meet the Town of Reading standards. All utilities shall be removed and reconnected from Washington Street. Electric, telephone and communication will also be extended from Washington Street. Natural gas connections shall be extended from Washington Street in coordination with respective utility companies and the Town. All proposed utilities will be underground.
- 3) **RMLD:** The electric utility plan, including transformer location and any proposed solar PV or high efficiency energy systems, shall be coordinated with and approved by the Reading Municipal Light Department (RMLD).
- 4) **Rooftop Mechanicals:** Mechanical units located on the roof shall be placed so that they are not viewable from the street level or abutting residential properties.
- 5) **Lighting:** If commercial is proposed, lighting shall be incorporated on the building to distinguish commercial entries from residential entries, as well as to provide a measure of public safety. Upper floor exterior lighting shall be designed to be Dark Sky compliant and mitigate impacts to abutting residential properties. The Applicant shall ensure that any proposed lighting is not occluded by the street trees along the frontage, and does not compete with existing street lighting. The Applicant shall submit specifications for each type of lighting fixture to the Community Development Director for approval.
- 6) **MS4 Permit:** The project shall comply with the most recent MS4 permit requirements.
- 7) **Drainage and Grading:** The site is relatively flat in grade and is currently 100% impervious due to the existing building and its associated parking. Redevelopment shall incorporate Best Management Practices (BMP's) and Low Impact Design (LID) strategies.

The site shall be graded in a manner to avoid puddling on the premises and to promote positive sheet flow away from the building.

Once the final stormwater system design is complete a long-term Operations and Maintenance Plan shall be prepared. The Plan shall detail measures to be taken by the property owner to ensure long-term sustainability of the system. The Plan shall include, but not be limited to: schedules for inspections and maintenance, estimated costs of maintenance, safety measures, and responsible entity. A separate construction phase BMP plan shall be drafted and followed.
- 8) **Sidewalk Improvements:** The existing sidewalk shall be replaced with new concrete sidewalk around the entirety of the site, and shall include 6" vertical granite curbing that lines the street.
- 9) **Bicycle Parking:** Bicycle parking shall be provided on-site or in an on-site garage.
- 10) **Loading / Deliveries:** Front door and on-street deliveries are not allowed to occur on Main Street or Washington Street. If commercial space is proposed, loading for commercial deliveries shall be managed on-site. The same is expected for both residential move-ins/outs, which shall be managed and scheduled by the property management company.
- 11) **Commercial Spaces:** It is strongly recommended that the Applicant prep any commercial space(s) with utility connections, grease traps, etc. in anticipation of future tenants.
- 12) **Limitations / Future Uses:** The 40R Development Plan Decision herein does not include approval for any future uses or site renovations that may – on their own merits and design – trigger the requirements of 40R plan review, or site plan review, and/or require a special permit. Pursuant to Section 10.5 of the Zoning Bylaw the following uses are permitted by right within the proposed commercial spaces: office, retail, restaurant, institutional and

consumer services.

Prior to Application for a Building Permit – Process for Staff Review of 40R Plan:

- 1) **Plan Submission:** In accordance with the Downtown Smart Growth District 40R Plan Application and Checklist, the Applicant shall submit all required documentation for a 40R Plan Approval to the Community Development Director.
- 2) **Compliance with ZBL Section 10.5:** The proposed 40R Plan shall comply with all provisions of Reading Zoning Bylaw Section 10.5 that constitute a by-right project, with the exception of the density waiver allowing for up to but no more than 5 residential units. No other waivers shall be sought or granted. Planning staff shall review and determine compliance; the Applicant shall submit revised plans if needed for compliance.
- 3) **Compliance with DSGD Design Guidelines:** The proposed 40R Plan shall comply with all provisions of the Downtown Smart Growth District Design Guidelines. No waivers shall be sought or granted. Planning staff shall review and determine compliance; the Applicant shall submit revised plans if needed for compliance.
- 4) **Development Review Team (DRT) Staff Review:** The Applicant and development team shall attend a Development Review Team (DRT) meeting to present the project to staff and receive comments and feedback.
- 5) **Plan Changes:** The Applicant shall submit revised plans as needed to comply with life safety codes, utility requirements, and engineering practices.
- 6) **DRT Staff Sign Off:** Once the DRT Staff are satisfied that the proposed 40R Plan complies with all bylaws, guidelines, codes, regulations, requirements and practices, Staff shall provide an official Sign Off of the 40R Plan.
- 7) **Time Limit/Bond:** The Plan Approval shall remain valid and run with the land indefinitely, provided that construction has commenced within two years after the decision is issued, which time shall be extended by the time required to adjudicate an appeal and which time shall be extended if the project proponent is actively pursuing other required permits or there is other good cause for failure to commence. The Approving Authority may require the posting of a performance bond to secure and/or screen a Development Project site in the event that demolition is undertaken but subsequent work lapses, for any reason within or outside the applicant's control, for a period longer than one year.

Prior to the Start of Any Construction (including Foundation):

- 1) **Building Permit Plans:** Upon completion of the Staff Review, the Applicant shall submit full building permit plans in accordance with the 40R Plan reviewed and signed-off on by Staff.
- 2) **Other Permits:** The Owner/Applicant is responsible for obtaining all other requirements and permits including but not limited to, utility connections, sewer, water, curb cut, street opening and Jackie's Law excavation permits from the Engineering Department (prior to excavation), and Board of Health approvals.
- 3) **Pre-Construction Meeting:** The Owner/Applicant and contractors shall coordinate with the Community Development Director to schedule a pre-construction meeting with Town staff prior to applying for demolition and/or building permits, in order to review these conditions and any and all final construction sequencing, details and plans for this project.

- 4) **Construction Management Plan / Contractor Parking:** The Applicant shall submit a Construction Management Plan which includes provisions for off-site parking, and shall include the following considerations:
 - a. **Construction Vehicle Restrictions:** The Applicant/Owner shall include language within their contracts prohibiting construction vehicles from entering/exiting the site from/onto Washington Street. Construction vehicles shall be directed to enter the site from Main Street and exit the site onto Main Street and travel northbound to the highway interchange.
 - b. **Road Lane Closures:** The Applicant/Owner shall include language within their contracts to ensure that no construction deliveries which require lane closure on Main Street occur during peak travel hours (8:00AM-10:00AM/4:00PM-6:00PM). At no point during construction shall both northbound travel lanes on Main Street be closed.
 - c. **Sidewalk Closures:** During times of sidewalk closures and/or pedestrian re-routing, the Applicant/Owner shall ensure that signage to the effect of “Sidewalks Closed, Businesses OPEN” are strategically placed near the site.
- 5) **Test Pits:** The Owner/Applicant shall perform soil test pits to confirm soil conditions and recharge rates. If changes are needed to the stormwater management infrastructure based on test pit findings the Owner/Applicant shall provide revised plans and details to the Town Engineer and the Community Development Director for review and approval.
- 6) **Construction Documents & Fire Safety:** Full construction documents must be submitted and approved by the Fire Department at 80% design. A fire flow test shall also be performed prior to the issuance of a building permit. A building permit shall not be issued until the Fire Department has approved the plans and test findings.
- 7) **Master Box:** The Applicant shall coordinate with the Fire Department on the requirement for a Master Box that is to be tied to the fire alarm system.
- 8) **Materials:** The Applicant shall submit samples of proposed materials and colors to be used on the building prior to installation, for review and approval by the Community Development Director.

After Foundation and Prior to the Start of Vertical Construction:

- 1) **Garage As-Built:** If a parking garage or podium is included in the project, the Applicant shall provide, to the Building Commissioner and Community Development Director, an as-built of the foundation and garage, that shows an overlay of the location and size of structural columns, fire/building/energy code requirements, and dimensioned parking striping, proving that the parking as approved can work. If the parking cannot work as signed off on by Staff, the Applicant shall submit revised plans to the Community Development Director and seek an amendment from Staff prior to starting vertical construction.
- 2) **Funding for Intersection Improvements:** In recognition of the fact that this project will add traffic to an already congested area with problematic intersections, the Applicant has agreed to contribute \$25,000 to the Town for the purpose of implementing recommendations within the peer review conducted and provided by Green International Affiliates, Inc. This funding shall be managed by the Town Engineer and/or Community Development Director for such improvements that are deemed necessary.

During Construction:

- 1) **Construction Hours:** Construction shall be limited to the hours stated in Section 8.9.8 “Construction Hours” of the Reading General Bylaws and said hours shall be posted in a conspicuous place at the entrance prior to any work on the site.
- 2) **Construction Activities:** Construction activities shall be conducted in a workmanlike manner at all times. Blowing dust or debris shall be controlled by the Applicant through stabilization, wetting down, and proper storage and disposal methods, subject to the approval of the Health Agent or designee. The Applicant shall ensure that the abutting local streets are kept clear of dirt and debris, which may accumulate as a result of construction activities for the Project. Documentation shall be provided demonstrating ongoing pest management control, subject to the approval of and administration by the Health Agent.
- 3) **Construction Management Plan / Contractor Parking:** Site operations shall comply with the aforementioned Construction Management Plan at all times. Contractors shall park in the locations designated and provided for within the CMP.
- 4) **Site Inspections:** Town staff or their designee shall have reasonable access to inspect the site to determine compliance with this Decision.
- 5) **Bond:** The Applicant/Owner shall furnish a bond for the final As-Built plans prior to the issuance of the final certificate of occupancy. The bond amount shall be determined by the Town Engineer consistent with the reasonable costs associated with a third party performing the work. The bond shall be returned once the requirements of this condition are met.
- 6) **Scaffolding:** Any scaffolding at the property lines shall be completely screened 100% of the time to maintain privacy and prevent materials/debris from falling/blowing or otherwise dropping onto the abutting properties.

Prior to the Issuance of a Certificate of Occupancy:

- 1) **Architecture:** The building façade on each elevation (north, south, east, and west) shall be substantially as indicated on the approved architectural plans and elevations.
- 2) **Stormwater O&M Plan:** An Operations and Maintenance Plan for the stormwater system shall be provided to the Town Engineer.
- 3) **Traffic Signal Phasing and Optimization:** The Applicant shall engage with the Town’s Engineering Department to review and optimize traffic signal timing based on projected traffic volumes prior to the occupancy of the development. The traffic signal timing shall be reexamined 6-months after occupancy for further necessary adjustments.
- 4) **Property Management / Leasing Documents:** A copy of the finalized Property Management / Leasing Documents shall be submitted to the Community Development Director for review and approval, and shall contain the following language:
 - a. **Fire Safety:** Language ensuring fire safety by prohibiting gas/propane grills on any balconies, etc.
 - b. **Delivery Vehicles & Times:** Language prohibiting commercial deliveries along Main Street and Washington Street, and prohibiting commercial deliveries to any commercial spaces between 10:00 PM and 5:00 AM.
 - c. **Management of Move-ins & Move-outs:** Language regarding management of move-ins and move-outs by the on-site property manager, specifically with regards to the

size of moving vehicles allowed and the timing and use of any garage or on-site loading area.

- d. **Drainage System Maintenance:** Language that requires the property management company to adhere to the requirements of the O&M Plan.
 - e. **Trash Removal:** Language detailing how trash and recycling will be managed on-site, including but not limited to schedule of pick-up days and times, and logistics for trash truck access to the site. Trash management for residential and retail uses shall be managed separately, as is practicable.
 - f. **Snow Removal:** Language detailing how snow will be managed and removed from the property, including the roof, and that snow storage shall not impact sight lines for vehicular traffic.
 - g. **Site Lighting:** Language that any commercial lighting (including for signage) shall be programmed to shut off at the close of business each day.
 - h. **Conditions for Ongoing Maintenance after Occupancy:** Language that the property management company shall adhere to the “Conditions for Ongoing Maintenance after Occupancy” as are stated herein below.
- 5) **Rooftop Mechanicals:** All rooftop mechanicals shall be set back from building facades and appropriately screened from view.
 - 6) **Pedestrian Improvements:** Improvements along Main Street, Washington Street and abutting rights-of-way, as deemed necessary or advantageous to the Town Engineer and Community Development Director, to adjacent crosswalks, signal heads, curbing and sidewalks, shall be installed at the Applicant’s expense in accordance with Town standards.
 - 7) **Garage Striping:** If proposed, garage parking spaces shall be striped in accordance with the approved plans. Dimensions shall be measured from centerline to centerline. Striping of parking lanes, and directional arrows, consistent with current Manual on Uniform Traffic Control Devices (MUTCD), shall be installed within the garage.
 - 8) **I/I Fee:** The Applicant is subject to the required Inflow/Infiltration Fee as the proposed sewer flow usage will be greater than historical usage. The Fee is calculated as twice the flow times \$4.00.
 - 9) **Street Trees:** If street trees are proposed, the Applicant shall work with the Tree Warden. Both the species and location shall be approved by the Town Tree Warden.
 - 10) **Lighting:** All exterior building and site lighting shall comply with the dark sky initiatives (light shall shine down only) with the light source being fully shielded (with cutoff shields) so that little to no light or glare spills onto abutting properties. Spec sheets of proposed lighting fixtures shall be submitted to the Community Development Director for review and approval.

Conditions for Ongoing Maintenance after Occupancy:

- 1) **Parking Utilization Data:** The Applicant shall provide reports to the Community Development Director indicating utilization of the on-site parking.
- 2) **Energy Utilization Data:** The Applicant shall provide reports to the Community Development Director detailing the building’s annual energy use and corresponding greenhouse gas emissions. A report shall be created utilizing the building’s benchmark year and Owners shall report energy use for all metered uses to the Town.

- 3) **Signage:** Prior to installation of any building or tenant signage, a Sign Permit Application and/or Master Signage Plan shall be submitted for review and approval.
- 4) **As-Built Plans:** Two full size paper copies and electronic AutoCAD final As-Built plans showing the building footprint, drainage systems and utility connections shall be submitted to the Community Development Director and Town Engineer to ensure compliance with this decision and other applicable Town standards. The bond held for this requirement will be returned to the Applicant once this condition has been fulfilled.
- 5) **Landscaping:** Landscaping on-site shall be maintained in a healthy condition in perpetuity. In the event that landscaping is damaged during snow removal operations, the property owner shall replace such landscaping during the next growing season.
- 6) **Lighting:** All exterior building and site lighting shall comply with the dark sky initiatives (light shall shine down only) with the light source being fully shielded (with cutoff shields) so that little to no light or glare spills onto abutting properties. Any exterior lighting that is required for security purposes may be illuminated by photocells and is not required to be extinguished at the close of business. All site and building lighting for commercial purposes, beyond what is needed for security purposes, shall be programmed to shut off at the close of business each day.
- 7) **Trash Removal:** All trash collection and disposal are the responsibility of the owner / property manager. The Applicant shall ensure daily that exterior areas of the site remain clear of debris, trash and any equipment used in connection with any commercial activities on site.

Plan Changes after Approval by the Commission:

Contemplated future changes to the plan approved herein shall be presented to the Community Development Director and the Building Inspector, or other relevant Town staff, for review prior to implementation of proposed changes.

10.5.13.1 Minor Plan Changes: After Plan Approval, an Applicant may apply to make minor changes in a Development Project involving minor utility or building orientation adjustments, or minor adjustments to parking or other site details that do not affect the overall build out or building envelope of the site, or provision of open space, number of housing units, or housing need or affordability features. Such minor changes must be submitted to the Commission on redlined prints of the approved plan, reflecting the proposed change, and on application forms provided by the Commission. The Commission may authorize such changes at any regularly scheduled meeting, without the need to hold a public hearing. The Commission shall set forth any decision to approve or deny such minor change by motion and written decision, and provide a copy to the Applicant for filing with the Town Clerk.

10.5.13.2 Major Plan Changes: Those changes deemed by the Commission to constitute a major change in a Development Project because of the nature of the change in relation to the prior approved plan, or because such change cannot be appropriately characterized as a minor change as described above, shall be processed by the Commission as a new application for Plan Approval pursuant to Section 10.5.

Appeal:

Any person aggrieved by this Decision of the CPDC may appeal to the appropriate court in accordance with the provisions of M.G.L. Ch. 40A Section 17, pursuant to M.G.L. Ch. 40R Section

11, within twenty (20) days after the date of filing of this Decision with the Town Clerk. Notice of any appeal with a copy of the complaint must also be filed with the Town Clerk within such twenty (20) days as provided in M.G.L. Ch. 40A Section 17.

This Decision and the relief, terms, restrictions and conditions contained herein shall run with the land and all subsequent owners shall benefit from and be bound by the relief, terms, restrictions and conditions contained herein.

Signed as to the accuracy of the vote as reflected in the minutes:

Andrew MacNichol, Staff Planner
Cc: Applicant, Town Clerk, DRT Staff, planning file

Date

DRAFT



Town of Reading
16 Lowell Street
Reading, MA 01867

Andrew MacNichol
Senior Planner
Phone: 781.942-6670
Fax: 781.942-9071
Website: www.readingma.gov

September 12, 2022

Downtown Smart Growth District (DSGD) Plan Review
M.G.L. Chapter 40R
DECISION

Project: 459 Main Street
Applicant: GC Fodera Contracting, Inc.

To the Town Clerk:

This is to certify that, at a public hearing of the Community Planning and Development Commission opened on February 7, 2022, discussed on March 14, 2022, April 11, 2022, May 16, 2022, June 13, 2022, August 8, 2022, September 12, 2022 and closed on XXX by a motion duly made and seconded, it was voted:

“We, the Reading Community Planning and Development Commission, upon request from the GC Fodera Contracting, Inc., under Section 10.5 of the Zoning Bylaws of the Town of Reading, and MGL Chapter 40R, to consider the application for 40R Development Plan Review to construct a 4-story mixed-use building with 12 housing units, and approximately 1,455 square feet of interior commercial space with an at grade garage containing 20 parking spaces, at 459 Main Street (Assessors Map 17, Lot 62) – as shown on the architectural plans prepared by Olinger Architects and the site plans prepared by Fodera Engineering, and listed below – do hereby vote XXX, to _____ the 40R Development Plan, inclusive of the listed waivers, subject to the Findings and Conditions below.”

Materials Submitted:

The following materials were submitted into the public record:

- a) Certified List of Abutters, dated 11/3/21;
- b) DSGD Development Application Form, dated 12/6/21;
- c) 459 Main Street Redevelopment Packet including: Project Narrative; Design Guideline Summary; Requested Waivers; and Land Owner Authorization, dated 12/6/21;
- d) Legal Notice, published in the Daily Times Chronicle on 1/20/22 and 1/27/22, and posted with the Town Clerk on 1/20/22;
- e) Adjustment of Public Hearing Notice, published in the Daily Times Chronicle on 8/24/22 and 8/31/22, and posted with the Town Clerk on 8/24/22;
- f) Civil Engineering Plan Set for 459 Main Street Proposed 40R Development, Reading, MA, prepared by Fodera Engineering, Inc., consisting of:

- a. Sheet C-1: Existing Conditions Survey Plan, dated 12/1/21;
- b. Sheet C-2: Proposed Site Plan, originally dated 12/1/21, and most recently revised 9/7/22;
- c. Sheet C-3: Proposed Utility Plan, originally dated 12/1/21, and most recently revised 9/7/22;
- d. Sheet C-4: Details Sheet, originally dated 12/1/21, and most recently revised 9/7/22;
- e. Sheet C-5: Details Sheet, originally dated 12/1/21, and most recently revised 9/7/22;
- f. Sheet C-6: Vehicle Tracking, originally dated 12/1/21, and most recently revised 9/7/22;
- g) Architectural Plan Set for 459 Main Street Redevelopment, Reading, MA, prepared by Olinger Architects and prepared for GC Fodera Contracting Company, consisting of:
 - a. Sheet G-000: Cover Sheet, dated 11/17/21, and most recently revised 4/5/22;
 - b. Sheet G-001: Project Information, Levels One through Four, dated 12/1/21, and most recently revised 3/7/22;
 - c. Sheet G-002: Zoning Guidelines, dated 12/1/21, and most recently revised 5/31/22;
 - d. Sheet G-003: Shadow Study, dated 12/1/21, and most recently revised 1/18/22;
 - e. Sheet G-004: Project Views, dated 12/1/21, and most recently revised 4/5/22;
 - f. Sheet G-005: Ground Level AXO, dated 12/1/21, and most recently revised 5/31/22;
 - g. Sheet G-006: Project Materials Board, dated 12/1/21, and most recently revised 1/18/22;
 - h. Sheet G-007: Context Elevations, dated 12/1/21, and most recently revised 1/18/22;
 - i. Sheet A-001: Site Plan, dated 12/1/21, and most recently revised 3/7/22;
 - j. Sheet A-002: Exterior Area Detail Plans, dated 12/1/21, and most recently revised 5/31/22;
 - k. Sheet A-100: Basement Plan, dated 12/1/21, and most recently revised 3/7/22;
 - l. Sheet A-101: Level 01 Plan, dated 12/1/21, and most recently revised 9/7/22;
 - m. Sheet A-102: Level 02 Plan, dated 12/1/21, and most recently revised 5/31/22;
 - n. Sheet A-103: Level 03 Plan, dated 12/1/21, and most recently revised 1/18/22;
 - o. Sheet A-104: Level 04 Plan, dated 12/1/21, and most recently revised 1/18/22;
 - p. Sheet A-105: Roof Plan, dated 12/1/21, and most recently revised 1/18/22;
 - q. Sheet A-300: Composite Elevations, dated 12/1/21, and most recently revised 5/31/22;
 - r. Sheet A-310: West Building Elevation, dated 12/1/21, and most recently revised 3/7/22;
 - s. Sheet A-312: South Building Elevation, dated 12/1/21, and most recently revised 5/31/22;
 - t. Sheet A-313: East Building Elevation, dated 12/1/21, and most recently revised 5/31/22;
 - u. Sheet A-314: North Building Elevation, dated 12/1/21, and most recently revised 4/5/22;
 - v. Sheet A-315: Corner and Misc. Elevation, dated 12/1/21, and most recently revised 1/18/22;
 - w. Sheet A-400: Building Sections, dated 12/1/21, and most recently revised 4/5/22;
 - x. Sheet A-401: Wall Sections One, dated 12/1/21, and most recently revised 5/31/22;
 - y. Sheet A-402: Wall Sections Two, dated 12/1/21, and most recently revised 4/5/22;
 - z. Sheet A-500: Stair Section One, dated 12/1/21, and most recently revised 1/18/22;
 - aa. Sheet A-501: Stair Section Two, dated 12/1/21, and most recently revised 1/18/22;
- h) Stormwater Management Report, 459 Main Street Redevelopment, prepared by Fodera Engineering, dated 12/2/21;
- i) 459 Main Street, Project Manual:
 - a. Traffic Impact Assessment, prepared by TEC, dated 11/17/21, and most recently revised 3/7/22;
 - b. Supplemental Traffic Impact Assessment, prepared by The Engineering Corp, dated 6/1/22;
 - c. Revised Trip Distribution Gravity Model – Residential, provided by The Engineering Corp, received 9/7/22;
 - d. Photometric Analysis, prepared by DIALux, dated 11/16/21;
- j) Summary Letter to CPDC, provided by Regnante Sterio LLP, dated 3/7/22;
- k) Summary Letter to CPDC, provided by Regnante Sterio LLP, dated 4/6/22;

- l) Summary Letter to CPDC, provided by Regnante Sterio LLP, dated 6/1/22;
- m) Summary Letter to CPDC, provided by Regnante Sterio LLP, dated 8/8/22;
- n) Responses to Town Engineer Memos, provided by Fodera Engineering, dated 6/1/22;
- o) 459 Main Street, Equinox Shadow Study Animation, received 6/1/22;
- p) Draft Construction Management Plan, provided by Genesis Construction and Development, dated 6/1/22, and most recently revised 8/1/22;
- q) 459 Main Street Snow and Ice Removal Plan, dated May 2022;
- r) Proposed Traffic Management Plan, received 8/1/22;
- s) Transportation Peer Review of Proposed Mixed-Use Development at 459 Main Street, performed by Green International Affiliates, Inc., dated 8/2/22;
 - a. Subsequent Letter of Review to Applicant Responses, dated 8/18/22
- t) Staff Reports:
 - a. Email from Reading Fire Department, dated 2/3/22;
 - b. Email from Reading Police Department, dated 2/4/22;
 - c. Memo from Town Engineer, dated 4/6/22;
 - d. Memo from Town Engineer, dated 5/12/22;
 - e. Email from Building Commissioner, dated 6/7/22.
 - f. Email from Fire Chief, dated 6/8/22.
 - g. Email from Economic Development Director, dated 8/2/22;
 - h. Email from Reading Fire Department, dated 8/2/22;
 - i. Email from Reading Police Department, dated 8/3/22;
 - j. Email from Assistant DPW Director, dated 8/3/22;
- u) Abutter Input:
 - a. Email from Ilene Bornstein, dated 2/8/22;
 - b. Email from Bob Kaminer, 37 Warren Avenue, dated 2/11/22;
 - c. Letter from Johnson Family, 166-168 Washington Street, received 3/15/22;
 - d. Email from Johnson Family, 166-168 Washington Street, received 5/4/22;
 - e. Letter from Johnson Family, 166-168 Washington Street, dated 6/13/22
 - f. Letter from Johnson Family, 166-168 Washington Street, re: Traffic Concerns, dated 7/11/22;
- v) Draft Decision, dated 9/12/22.

General Findings:

- 1) Zoning: The site is located within the underlying Business-B Zoning District and the Downtown Smart Growth District (DSGD) / 40R Overlay District. Properties located to the north, south, and west are also located in the Business-B and DSGD Zoning Districts. The lot abutting the site to the east is split-zoned between the Business-B/DSGD Zoning District and the S-15 Residential Zoning District.

The site is considered a Transitional Area (directly abutting a lot containing two-family dwelling).

→Therefore, Section 10 of the Design Guidelines applies to this redevelopment.

- 2) Overview: The land totals 10,256 square feet in area and maintains a combined ~192.57 linear feet of frontage along Main Street to the west and Washington Street to the south. The site is abutted by: an existing 40R Mixed-Use Development to the north; a two-family dwelling to the east; Washington Street to the south; and Main Street to the west.

The existing site contains an ~1,800 square-foot, single-story commercial structure that operates as an automotive repair facility. It also maintains three curb cuts (one along Main Street and two along Washington Street) and an associated at-grade parking area.

The Applicant proposes to redevelop the site into a Mixed-Use 40R Development including twelve (12) residential dwelling units. It will also include a total of 1,455 square-feet of interior commercial space on the first-floor, and twenty (20) associated parking spaces located within a garage built at-grade. Vehicles would access the garage via a 24-foot wide drive-entry along Main Street and exit through a 22-foot wide drive-exit along Washington Street. The garage totals 6,548 gross square-feet of area, which equates to 72.5% of the at-grade area.

The Applicant is seeking Development Plan approval from the CPDC under Section 10.5 of the Reading Zoning Bylaw and the Downtown Smart Growth District Design Standards & Guidelines, pursuant to M.G.L. Chapter 40R.

- 3) Setbacks, Step-backs and Dimensional Requirements: A one-story roofed structure is setback 0' from the northern lot line in order to enclose a series of parking stalls along the north end of the garage. The second-, third-, and fourth-stories of the building's north façade will be stepped-back 3'6" from the northern lot line.

The building structure maintains a 16' setback from the eastern lot line where the project abuts a split-zoned (Bus-B/S-15) lot containing an existing two-family structure with a detached commercially-used outbuilding. An 8' tall and 87'-4" long perimeter fence shall be located along the eastern property line to screen the open parking area. Additionally, the fourth-floor of the building's east façade is stepped-back 4'-2" from the levels below to further mitigate the massing of the structure for a total step-back of 20'-2" from the eastern lot line to the fourth-floor level.

Projecting bays are utilized over building step-backs on the building's south and west façades. A 0'-9-10" setback is provided from the southern lot line for a 7' concrete sidewalk along Washington Street. The building also maintains a 1'-7"-2' setback from the western lot line to allow for a wider (6'-6"-7') sidewalk area along Main Street. Additional setback areas of 6' are located along the retail portions of Main Street and Washington Street to allow for sidewalk area up to ±12' wide for additional programming and/or landscaping opportunities.

There is also a 9' X 15' setback area at the southeastern corner of the development. This corner shall be treated with landscaping that does not infringe on sight lines for the abutting property owners when they exit their driveway.

Building Height: For structures with flat roofs, 'height' is defined in Section 2.0 of the Reading Zoning Bylaw as "*The vertical distance from the average grade around the perimeter of a building to the top of a flat roof, including any parapet...*" Height is not defined separately or differently for 40R projects within ZBL Section 10.5, and mixed-use 40R projects are allowed a maximum height of 45' unless a height waiver can be justified.

Section 10.4.1 of the Design Guidelines requires the following: "*building height shall be measured from the pre-development site grade.*" The maximum building height, to the parapet, based off of the pre-development site grade, is 45'. [While not counting towards

maximum height requirements, it should be noted that the elevator penthouse measures 7' in height and the stairwell measures 9' in height bringing maximum building height to 54'.]

Design of a flat roof with a second empire/slanted mansard design is utilized. The elevator shaft and mechanical utilities shall be located on the roof and be setback and/or screened from view. A total of 2,016sf of roof area shall be designated for the potential of future install of photovoltaic solar panels.

Lot Coverage: The lot totals ~10,256 square-feet of area, 7,870 square-feet of which will be covered by the principal structure. This calculates to a lot coverage of 77.2%.

- 4) Interior/Exterior Space: The proposed project will comprise +/-23,985 net enclosed square feet, as follows: ±1,378 (basement); +/-2,488 (1st floor); +/-7,143 (2nd floor); +/-7,137 (3rd floor); +/-5,540 (4th floor); and +/-0 (roof). The basement area shall be dedicated to utility space. The first-floor/garage level includes twenty (20) parking spaces dedicated to but unbundled from the residential use, along with a trash room, storage room, bicycle parking, an elevator system, a mail room, a utility room, two residential lobbies, and the retail space. The storage space within the first-floor will also be provided for bicycle parking and battery storage for the rooftop solar. However, the storage areas shall not be roofed. Residential entries will be provided on both Main Street and Washington Street.

The 1,455 square foot commercial space will include two pedestrian entries on the corner of Main Street and Washington Street that will provide access to and from expanded 12'-wide sidewalk areas located on both Main Street and Washington Street. The expanded sidewalk area is expected to be used for additional programming and landscaping.

The residential units include a mix of one-, two- and three-bedroom units. Five out of the six units located on the third-floor are to be bi-level units with a second level on the fourth-floor. A total of four (4) one-bedroom apartments, seven (7) two-bedroom units, and one (1) three-bedroom will be provided. One-bedroom units average 949 gross square feet, two-bedroom units average 1,472 gross square feet and the three-bedroom unit totals 2,107 gross square-feet.

A 662 square foot terrace fronting the corner of Main Street and Washington Street, which overlooks Elm Park, shall be located at level four, and will be available to all residents of the building. Access to it will be provided through the residential stairwell, which is accessible from all floors of the building.

All residential units will have private balconies or access to outdoor terraces. Sizes will vary in both width and length. No balcony or terrace shall be located on the building's north façade. There are no enclosed areas on the roof and there will be no public access to the roof.

Any concerns on bike parking and solar storage not being covered by roof?

- 5) Roof: Mechanical units located on the roof shall be placed so that they are not viewable from the street level or abutting residential properties. The elevator shaft is approximately 7' tall and the stair penthouse is 9' tall. Approximately 2,016 square-feet of roof area shall be dedicated to for the potential of future solar array install.

- 6) Commercial Space: The project proposes approximately 1,455 square feet of interior commercial space on the first-level as described above. The commercial space shall also be provided with sidewalk area up to ±12' wide to utilize for outdoor programming. Commercial space is expected to be utilized as restaurant or retail. Three (3) parking spaces within the garage shall be dedicated to the commercial use. While one of the three parking spaces dedicated to commercial use meets the requirements of a standard ADA space it shall not be signed or restricted as such (i.e., ADA-adaptable) for flexible use.
- 7) Parking: The project requires a minimum of fifteen (15) parking spaces for the twelve units. There are twenty (20) parking spaces in the garage. Three (3) parking spaces shall be dedicated to the commercial use and the remaining seventeen (17) spaces shall be dedicated to, but unbundled from, the residential units. The garage shall be accessed through a 24' wide curb cut on Main Street. Egress from the garage shall be provided via a 22' wide curb cut on Washington Street. The Washington Street exit shall be gated.

The three commercial parking spaces provided shall each measure 8'6" wide and 17' long. The westernmost commercial parking space will be next to a 5'2" clearance area in order to act as a standard ADA space when needed. The commercial parking spaces are the three most immediate spaces located along the northern end of the garage when entering from Main Street. It is expected that two of the commercial parking spaces will be utilized and signed for short-term customer parking and the remaining space to be dedicated to retail employee.

Seventeen (17) parking spaces will be provided for the residential units, which equates to 1.41 spaces per unit and 0.81 spaces per bedroom. Residential parking includes two (2) ADA spaces, one of which will be an ADA van accessible space that measures 12' wide and 17' long and one standard ADA space that measures 8'-6" wide and 17' long will be provided. A 5'-2" striped area between the two ADA spaces will meet clearance requirements. Eleven (11) standard spaces measuring 8'-6" wide and 17' long will be provided. Four (4) compact spaces measuring 8' wide and 17' long will also be provided along the garage's eastern end.

All parking spaces will be 'EV Ready' in that wiring required for Electric Vehicle charging stations shall be included during construction. The implementation of EV chargers will be up to individual unit owners who require such.

Commercial Parking: The site is NOT within 300' of a municipal lot and is NOT exempt from providing off-street commercial parking. However, because the proposed use is designed to be retail and/or restaurant, and is less than 2,000 square feet, zero (0) off-street parking spaces are required or provided per Reading Zoning Bylaw Section 10.5.8:

10.5.8.1 Off-Street Parking

Off-street parking shall be provided to meet the following minimum requirements:

Retail or Restaurant	0 spaces
Office and Institutional	2 spaces per 1,000 square feet

If an Office/Institutional use is proposed within the commercial space, two (2) parking spaces will be required for every 1,000 square feet. With the commercial space total of 1,455 square feet, three (3) off-street commercial parking spaces would be required. However, the Applicant is proposing to add three (3) additional off-street parking spaces within the garage for the commercial use. It should also be noted that there is no on-street parking along the property frontage on either Main Street or Washington Street.

Loading / Deliveries: Front door and on-street deliveries are not allowed to occur on Main Street or Washington Street. Loading is proposed to occur within the garage and includes access to the retail space. Commercial loading and deliveries will occur during off-peak traffic hours and the size and nature of the commercial space is expected to be served by box trucks and vans, and not trailer trucks. Commercial deliveries shall be provided access to the commercial area from within the garage. The same is expected for both residential move-ins/outs, which shall be managed and scheduled by the property management company.

With the commercial space total of 1,455 square feet, no formal dedicated off-street loading zone is required.

10.5.8.2 Off-Street Loading & Delivery

Off-street loading spaces shall be provided to meet or exceed the following minimum requirements:

Restaurant leasable space in excess of 2,000sf 1 space per 2,000sf

Other allowed Secondary Use leasable space in excess of 2,000sf 1 spacer per 5,000sf

Bicycle Parking: A series of bike racks will be installed within the garage.

- 8) **Sidewalk Improvements:** The existing sidewalk shall be replaced with new concrete sidewalk around the entirety of the site, and will include 6" vertical granite curbing that lines the street. The concrete sidewalk will vary from 6'6"-7' around the site and a 2'-3' brick paver band will line the exterior. An additional 6' of sidewalk depth will be provided along portions of the retail space to allow for public programming and additional landscaping opportunities. Curb cuts are to be consolidated from three to two.
- 9) **Traffic Flow and Volume:** A Traffic Impact and Access (TIA) study was completed for the project by The Engineering Corp (TEC).

The TIA concludes with the following information/recommendations:

- The proposed development is anticipated to generate approximately 138 new vehicle trips during the average weekday. 7 new vehicle trips are anticipated during weekday morning peak hour and 10 new vehicle trips are anticipated during the weekday evening peak hour.
- Overall, the project is anticipated to result in an increase of traffic on the roadway network by approximately 1 vehicle every 6 minutes; the report indicates this is a negligible impact.

- The 2028 Building Condition yielded similar delays and same Level of Service (LOS) results as the No Build Condition, indicating the proposed project will not significantly impact peak hour traffic operations. The delay is expected to slightly increase for several movements but the report indicates the overall impact of the increase in delays per vehicle will be negligible.
- The Applicant shall work with the Town of Reading to monitor and optimize traffic signal timings post-occupancy, if necessary, through joint field observations.
- The level of impact from the project does not warrant any additional project-specific mitigation beyond the signal timing optimization identified above.

10) Peer Review: A peer review of the project site plans, supplemental traffic impact information and site access and circulation was conducted by Green International Affiliates, Inc. The peer review included a series of recommendations not provided in the TIA aimed to improve the site circulation, access and safety.

11) Drainage and Grading: The site is relatively flat in grade and is currently 100% impervious area due to the existing building and its associated parking. Redevelopment will incorporate Best Management Practices (BMP's) and Low Impact Design (LID) strategies. LID measures include an infiltration system that improves the natural runoff rate over existing conditions.

Roof runoff will be captured and directed to the on-site infiltration gallery. Portions of the roof runoff not captured will flow onto the new sidewalk and towards existing conveyance systems on both Main Street and Washington Street. Treatment prior to infiltration will be completed through a manhole with deep sump prior to discharging into the recharge system. The deep sump will separate Total Suspended Solids (TSS) for advanced pollutant removal. The system will be designed to capture up to 100-year storm events. An overflow outlet pipe will be included in the system in case of an extreme event. Overflow will be directed toward Washington Street.

The site will be graded in a manner to avoid puddling on the premises and to promote positive sheet flow away from the building.

Once the final stormwater system design is complete, a long-term Operations and Maintenance Plan will be prepared. The Plan shall detail measures to be taken by the property owner to ensure long-term sustainability of the system. The Plan shall include, but not be limited to, schedules for inspections and maintenance, estimated costs of maintenance, safety measures, and responsible entity. A separate construction phase BMP plan shall be drafted and followed.

12) Utilities: All utilities are to be removed and reconnected from Washington Street. The existing sanitary sewer will be cut and capped at the 20" main within the right-of-way and replaced with a new PVC pipe. An oil-water separator is to be installed within the garage and will connect to the sanitary sewer line. Domestic water and a new fire service will be tapped from the 12" water main in Main Street. Electric, telephone and communication will also be extended from Washington Street. Natural gas connections will be extended from Washington

Street and shall be coordinated with respective utility companies. All proposed utilities will be underground.

A total of 2,016sf of roof area is dedicated for future photovoltaic (PV) solar arrays. The area would allow an estimated 69 PV panels and a bank of PV batteries will be installed within the building's garage that will modulate the building's energy demand with the grid.

- 13) Lighting: Lighting shall be incorporated at the first level façade in order to distinguish commercial entries from residential entries, as well as to provide a measure of public safety. Upper floor exterior lighting shall be designed to be Dark Sky compliant and mitigate impacts to abutting residential properties.
- 14) Property Management: The property is proposed to be managed by a property management company.
- 15) Wetlands / Floodplain: There are no wetland resource areas or buffers on or near the site, and the site is not within a 100-year floodplain.
- 16) Landscaping: A mulch bed with shrubs is proposed at the retail entrance. The existing street tree on Main Street is proposed to remain. Additional shrubs and vegetation will be provided in the southeast corner of the lot.
- 17) Trash Management: Trash and recycle bins shall be located within the garage and near the Main Street entry. Trash is to be managed by private entity. Language detailing how trash and recycling will be managed on-site, including but not limited to schedule of pick-up days and times, and logistics for trash truck access to the site shall be described within the property management documents. Trash management for both the residential and retail uses shall be managed separately, as is practicable.
- 18) Signage: No building signage is approved herein. Any future signage shall require the submittal of a Sign Permit Application and shall comply with Section 8.0 of the Zoning Bylaw and Section 9.0 of the Downtown Smart Growth District Design Guidelines.

Findings pursuant to DSGD Design Standards & Guidelines:

7. Building Design Standards

7.1 Massing

7.1.1 Front Façade Setback – Over 60% of the Main Street façade is at the minimum setback of 0'. Over 60% of the Washington Street façade is also at the minimum setback of 0'. Recessed retail areas of 6' will be provided on both the Main Street and Washington Street and shall be designed specifically for pedestrian uses.

7.1.2 Building Step-Back Requirements – The second- through fourth-floors of the building's north façade are stepped-back 3'6". Projecting bays are utilized on the southern and western façades.

Design Guidelines Section 7.1.2 Diagram D requires the following:

“Where buildings are directly abutting a single-family, 2-family or 3-family residential use, the side yard or rear yard step-back shall be at the 2nd or 3rd floor and shall be a minimum of ten (10) feet.”

The eastern property line directly abuts a 2-family use. The fourth-floor of the proposed building on the east façade is stepped back 4'2"; no step backs are currently proposed at the 2nd and 3rd floors of this façade as required.

→ Based on prior feedback from the CPDC and abutters, the Applicant modified the project to remove the enclosed garage and the step-back which complied with Design Guidelines Section 7.1.2 Diagram D. This change means that the project no longer complies with this requirement; staff recommend that the Applicant seek a waiver from this requirement.

→ In addition, staff recommend that the Applicant provide an analysis of the north façade and seek waivers for any deviations from the Design Guidelines.

7.1.3 Mixed-Use Building Proportions – The building's commercial space and main entry space is provided horizontal brick work and large glass paned windows. Residential floors above are provided projecting bays, vertical windows and blue shipyard siding to differentiate from the retail uses below.

7.1.4 Special Function Space Differentiation – Not applicable to the current proposal; however, the Applicant is encouraged to think about community place-making events when approaching commercial tenants, especially for the second-floor retail space.

7.2 Appearance

7.2.1 Defined Proportions – The project uses projecting bays and balconies, and a second empire/mansard rooftop design to define different levels of the façade.

7.2.2 Horizontal and Vertical Elements – Horizontal elements such as brick masonry, retail awnings, and large framed windows are combined with vertical projecting bays, cornice roof designs at different levels, and mansard roofing on the fourth-floor. Materials shall be submitted to CPDC for review.

7.2.3 Continuous Façade Elements – Façade elements and materials are used continuously around the façade.

7.2.4 Rooftop Mechanical Setbacks – Mechanical units on the rooftop level are setback and screened so as to not be visible from the pedestrian level.

7.2.5 Incorporation of Existing Significant Building Facades – The building form adapts in scale and texture to create continuity with abutting properties.

7.2.6 Franchise Architecture – Distinctive building design that is trademarked or identified with a particular chain or corporation and is generic in nature, is not allowed in the DSGD – the Applicant shall be aware of this when recruiting tenants.

7.3 Entries

7.3.1 Articulation – Commercial entries are provided at the corner of Main and Washington Street and shall be articulated through design and signage. Residential entries shall be recessed and will be provided on both Main Street and Washington Street.

7.3.2 Retail and Commercial Entry Transparency – Commercial space is designed to activate Main Street and a portion of Washington Street. Commercial areas shall be visible from Elm Park across Main Street. Commercial spaces are provided large glass window panels for visibility.

7.3.3 Integrated Lighting & Signage – Sidewalk lighting has been designed around the entire perimeter of the building.

7.3.4 Upper Floor Entries – The entry to upper floor residential areas shall be made distinct upon entering through the either the Main Street or Washington Street façades by signage and design.

7.4 Fenestration

7.4.1 Commercial Horizontality & Residential Verticality – Commercial spaces are designed with over 60% of their façade length being glass window panels. Residential portions of the building are designed with vertical windowing.

7.4.2 Glazing – Commercial spaces are designed with over 60% of their façade length being glass window panels. Retail or restaurant uses were stated as intended tenants to provide activation of the areas and limit tint of the windows.

7.4.3 Overhanging Awnings or Canopies – Awnings to distinguish the commercial spaces shall be utilized.

7.5 Materials

7.5.1 Exterior Finishes – A combination of light-colored masonry, blue ship-lap siding, patina finishing, bronze finishing and green slated shingles will be used for the building façades.

7.5.2 Prohibited Materials – Not Applicable.

7.5.3 Changes in Materials – The first-floor will consist of a light-colored masonry base to transition from the sidewalk to the building structure. Commercial spaces will utilize large windows inserted into the masonry storefronts. Residential spaces and entries above will differentiate themselves by utilizing a navy ship-lap siding, bronze and patina finishing, and a green slated shingled roof. Materials shall be submitted to CPDC for review.

7.5.4 Continuity of Materials – Façade elements and materials are used continuously around the façade.

7.5.5 Blank Facades Not Permitted – The second- and third-floors of the north façade utilize the navy ship-lap siding and the fourth-floor utilizes the green slated roof with a mansard design. No windows are proposed for this facade for privacy reasons due to the proximity of the adjacent residential building to the north.

→ Does this need a waiver?

8. Site Design Standards

8.1 Sidewalks

8.1.1 Sidewalk Continuity – The existing curb cuts on Washington Street will be consolidated from two to one. Widened sidewalks will be provided for pedestrian use.

8.1.2 Pedestrian Amenities – The project will provide ~500sf of at-grade open space accessible from the sidewalk (~900sf new sidewalk).

8.1.3 Usable Open Spaces – The open space will be programmed dependent on commercial uses, but will most likely be used for pedestrian-centric activities such as dining and seating.

8.1.4 Pedestrian Improvements – Improvements to adjacent crosswalks, curbing and sidewalks may be requested by the Town Engineer.

8.2 Driveways and Parking

8.2.1 Sidewalk Continuity – The existing sidewalk will be removed and replaced. Curb cuts on Washington Street will be consolidated from two to one.

8.2.2 Parking Lots – A majority of the parking garage shall be contained within the building structure and concealed from public view. The access will be provided off of

Main Street and exit will be provided along Washington Street. The parking area shall be screened by an 8' tall fence along the eastern lot line.

8.2.3 Parking Lots Behind Buildings – The ground level parking shall be screened by an opaque fence.

8.2.4 Below-grade Parking – Not Applicable to application.

8.2.5 Parking Lot Screening – The portion of the parking area abutting the existing two-family dwelling will be screened by an 8' opaque privacy fence. The fence shall screen the residential parking area for the residential abutter.

8.2.6 Shared Parking – Shared use of parking between residential and commercial tenants will be encouraged. If the parking garage is not fully utilized by residents the Applicant shall look to allow commercial employees garage parking access. Ride sharing services will be encouraged.

8.2.7 Pedestrian & Vehicular Safety – Recommendations of the Traffic Impact Assessment shall be considered and discussed with the Town's Parking Traffic and Transportation Task Force (PTTTF).

8.3 Landscaping

8.3.1 Street Trees – The existing street tree on Main Street is to remain.

8.3.2 Retail Frontages – The final location of street trees shall be determined by the Tree Warden and shall not impede visibility of commercial areas or signage.

8.3.3 Parking Areas – The eastern parking area will be screened by an 8' fence, the rest of the parking area shall be enclosed within the garage.

8.3.4 Public Open Spaces – The project will provide ~500sf of publicly accessible open space accessible from the sidewalk (~900sf new sidewalk). An additional 662sf of private amenity space for residents is provided at the 4th floor level.

8.3.5 Native Species – Final determination of street tree species shall be determined by the Tree Warden.

8.3.6 Preservation of Healthy 6" Caliper Trees – Not Applicable to application.

8.4 Lighting

8.4.1 Articulation of Building Uses & Entries – The project will incorporate lighting along the street level façade that will identify major commercial and residential entry ways. Any upper level lighting shall be Dark Sky compliant and designed to mitigate impact to residential abutters.

8.4.2 Coordination w/Town's Street Lighting & Trees – All proposed lighting will be coordinated with the Town's street lighting and street trees.

8.4.3 Light Spillover – Lighting at upper-level terraced areas shall be designed to minimize impact to abutting properties.

8.4.4 Public Safety – All lighting for public safety shall be added to the plans.

8.4.5 Sign Lighting – No signage proposed or approved herein.

8.4.6 Dark Sky Standards – All upper floor lighting shall comply with dark sky standards.

8.5 Utility Areas and Utilities

8.5.1 Location – The trash area and utility rooms will be located within the parking garage. The trash area will be situated along the northern façade. Mechanical units will be located on the roof, and will not be visible from the street.

8.5.2 Screening – All rooftop mechanicals will be setback so they are not viewable from street level.

8.5.3 Shared Utility Areas – Not Applicable.

8.5.4 Aboveground Utilities Not Permitted – All utilities will be underground.

8.5.5 Underground Utilities Required – All utilities will be underground.

8.6 Drainage and Storm Water Management

8.6.1 BMP/LID Strategies – Roof runoff will be captured and directed to the on-site infiltration gallery. Portions of the roof runoff not captured will flow onto the new sidewalk and towards existing conveyance systems on both Main Street and Washington Street. Treatment prior to infiltration will be completed through a man hole with deep sump prior to discharging into the recharge system.

8.6.2 System Elements – A series of underground infiltration systems are proposed beneath the garage floor; the system is designed to capture roof runoff and reduce flow into the Town's system.

8.6.3 Operations & Maintenance Plan – An O&M Plan shall be provided.

8.6.4 On-site Recharge – On-site recharge has been provided.

8.6.5 Pervious Paving – Not Applicable to application.

8.6.6 Site Grading – As existing, the site is proposed to remain relatively flat.

9. Signage Design Standards – No building signage has been proposed or approved herein.

10. Additional Considerations for District Edges & Transitional Areas

10.3 Applicability – The site is designated as a Transitional Area as it abuts an existing two-family structure.

10.5 Design Considerations for Transitional Areas

10.5.1 Abutting Historic Structures – Not Applicable to application.

10.5.2 Density of Project away from Residential Use – The fourth-floor is stepped back 4'2" from the eastern property line. Primary retail spaces are located at the western and southwestern facades of the building in order to add to the primary commercial street of Main Street.

10.5.3 Engage Existing Residential Fabric – The project utilizes a 'second empire' mansard roof design to engage existing residential fabric.

10.5.4 Screen for Residential Privacy – The portion of the parking area abutting the existing two-family dwelling will be screened by an 8' fence.

10.5.5 Shadow Study – A shadow study has been provided.

10.5.6 Noise Mitigation – Mechanical units located on the roof shall be placed and screened so that they are not heard from the street level or abutting residential properties.

Waivers pursuant to Section 10.5.12 and DSGD Design Standards & Guidelines:

Upon request of the Applicant, the Commission, in the interests of design flexibility and overall project quality, and upon a finding of consistency of such variation with the overall purpose and objectives of the DSGD and the Reading Master Plan, or if it finds that such waiver will allow the project to achieve the density, affordability, mix of uses and/or physical character allowed. The Commission shall take into consideration the following items when considering a waiver:

1. High performance energy efficient buildings and construction methods.
2. Projects with publicly accessible open space.
3. Projects that include retail and restaurants located on street level.
4. A demonstrated shared parking initiative that makes efficient use of land and existing parking supply.

5. The preservation or rehabilitation of historic properties or other buildings considered significant to the Town.

The Applicant has requested the following waivers from ZBL Section 10.5:

1. **Density:** *to allow a density of 51.0 units/acre where 20 units/acre is the maximum permitted.* The Applicant stated this will allow the development to be economically viable and notes that the Floor Area Ratio (FAR) of 2.72 is below the maximum of 2.80 allowed.
2. **Parking Dimensions:** *to allow 4 parking spaces to be striped, sized and signed as compact spaces that measure 8' wide and 17' long where 8.5' x 17' is permitted.* This totals 20% of the garage parking spaces.
3. **Garage Drive Aisle Width:** *to allow 24' drive aisle widths within the parking garage where 26' is required.* The Applicant states that 24-foot wide drive aisles may accommodate the one-way vehicular flow of traffic.
4. **Landscaping:** *to waive the requirement of submitting a full landscaping plan and provide landscaping where appropriate.*

The CPDC voted XXX to _____ the requested waivers.

Conditions:

General:

- 1) **Public Health, Safety and Welfare:** If, at any time, the site becomes a nuisance to public health, safety or welfare (i.e., traffic spillover, excessive noise, unreasonable site illumination beyond the hours of operation, etc.) – as shall be evidenced by substantiated complaints to the Police Department or Public Services Office – the Applicant/Owner shall agree to work with staff to rectify the problem. Should the situation warrant it, an additional Site Plan Review by the CPDC may be required.
- 2) **Utilities:** All utilities, structures, frames and covers shall meet the Town of Reading standards. The electric utility plan is subject to approval by the Reading Municipal Light Department (RMLD).
- 3) **MS4 Permit:** The project shall comply with the most recent MS4 permit.
- 4) **Lighting:** The Applicant shall ensure that any proposed lighting is not occluded by the street trees along the frontage, and does not compete with existing street lighting. The Applicant shall submit specifications for each type of lighting fixture to the Community Development Director for approval.
- 5) **Limitations / Future Uses:** The 40R Development Plan Decision herein does not include approval for any future uses or site renovations that may – on their own merits and design – trigger the requirements of 40R plan review, or site plan review, and/or require a special permit. Pursuant to Section 10.5 of the Zoning Bylaw the following uses are permitted by right within the proposed commercial spaces: office, retail, restaurant, institutional and consumer services.
- 6) **Commercial Spaces:** It is strongly recommended that the Applicant prep the commercial space(s) with utility connections, grease traps, etc. in anticipation of future tenants.
- 7) **Engineering Concerns:** In general, throughout the project, the Applicant shall work with the Town Engineer to address any outstanding comments in the memos to the Community Development Director dated 4/6/22 and 5/12/22.

- 8) **Shared Parking:** The Applicant is encouraged to engage in conversations with nearby property owners regarding shared parking, and to partner with Zip Car and other shared services if possible, and to provide electric vehicle charging stations. If and when progress on shared parking is made, the Applicant shall provide more information about these amenities, and indicate which area(s) of the garage are intended for them and how they will be managed.
- 9) **Storage Areas:** The Applicant is encouraged to consider adding supplemental storage areas for tenants to the building if possible.
- 10) **Community Place-making / Creative Economy:** When approaching potential commercial tenants, the Applicant is encouraged to think about community place-making events, and/or dividing the space into smaller units that are affordable to creative economy tenants.

Prior to the Issuance of Building Permits and Prior to the Start of Construction:

- 1) **The Applicant shall make the following plan changes, and shall submit two (2) full size (24x36) copies of the revised plans to the Community Development Director:**
- 2) **Other Permits:** The Owner/Applicant is responsible for obtaining all other requirements and permits including but not limited to, utility connections, sewer, water, curb cut, street opening and Jackie’s Law excavation permits from the Engineering Department (prior to excavation), and Board of Health approvals.
- 3) **Pre-Construction Meeting:** The Owner/Applicant and contractors shall coordinate with the Community Development Director to schedule a pre-construction meeting with Town staff prior to applying for demolition and/or building permits, in order to review these conditions and any and all final construction sequencing, details and plans for this project.
- 4) **Construction Management Plan / Contractor Parking:** The Applicant shall submit a Construction Management Plan which includes provisions for off-site parking.
- 5) **Test Pits:** The Owner/Applicant shall perform soil test pits to confirm soil conditions and recharge rates. If changes are needed to the stormwater management infrastructure based on test pit findings the Owner/Applicant shall provide revised plans and details to the Town Engineer and the Community Development Director for review and approval.
- 6) **Construction Documents & Fire Safety:** Full construction documents must be submitted and approved by the Fire Department at 80% design. A fire flow test shall also be performed prior to the issuance of a building permit. A building permit shall not be issued until the Fire Department has approved the plans and test findings.
- 7) **Master Box:** The Applicant shall coordinate with the Fire Department on the requirement for a Master Box tied that is to be tied to the fire alarm system.
- 8) **Materials:** No colors have been approved herein. The Applicant shall return to the Commission with samples of proposed materials and colors to be used on the building prior to installation.

During Construction:

- 1) **Construction Hours:** Construction shall be limited to the hours stated in Section 8.9.8 “Construction Hours” of the Reading General Bylaws and said hours shall be posted in a conspicuous place at the entrance prior to any work on the site.

- 2) **Construction Activities:** Construction activities shall be conducted in a workmanlike manner at all times. Blowing dust or debris shall be controlled by the Applicant through stabilization, wetting down, and proper storage and disposal methods, subject to the approval of the Health Agent or designee. The Applicant shall ensure that the abutting local streets are kept clear of dirt and debris, which may accumulate as a result of construction activities for the Project. Documentation shall be provided demonstrating ongoing pest management control, subject to the approval of and administration by the Health Agent.
- 3) **Construction Management Plan / Contractor Parking:** Site operations shall comply with the aforementioned Construction Management Plan at all times. Contractors shall park in the locations designated and provided for within the CMP.
- 4) **Site Inspections:** Town staff or their designee shall have reasonable access to inspect the site to determine compliance with this Decision.
- 5) **Bond:** The Applicant/Owner shall furnish a bond for the final As-Built plans prior to the issuance of the final certificate of occupancy. The bond amount shall be determined by the Town Engineer consistent with the reasonable costs associated with a third party performing the work. The bond shall be returned once the requirements of this condition are met.
- 6) **Scaffolding:** The scaffolding at the property lines shall be completely screened 100% of the time to maintain privacy and prevent materials/debris from falling/blowing or otherwise dropping onto the abutting properties.
- 7) **Construction Vehicle Restrictions:** The Applicant/Owner shall include language within their contracts prohibiting construction vehicles from entering/exiting the site from/onto Washington Street. Construction vehicles shall be directed to enter the site from Main Street and exit the site onto Main Street and travel northbound to the highway interchange.
- 8) **Road Lane Closures:** The Applicant/Owner shall include language within their contracts to ensure that no construction deliveries which require lane closure on Main Street occur during peak travel hours (8:00AM-10:00AM/4:00PM-6:00PM). At no point during construction shall both northbound travel lanes on Main Street be closed.
- 9) **Sidewalk Closures:** During times of sidewalk closures and/or pedestrian re-routing, the Applicant/Owner shall ensure that signage to the effect of “Sidewalks Closed, Businesses OPEN” are strategically placed near the site.

Prior to Vertical Construction:

- 1) **Garage As-Built:** The Applicant shall provide, to the Building Commissioner and Community Development Director, an as-built of the foundation and garage, that shows an overlay of the location and size of structural columns, fire/building/energy code requirements, and dimensioned parking striping, proving that the parking as approved can work. If the parking cannot work as approved, the Applicant shall return to CPDC for an amendment prior to starting vertical construction.
- 2) **Funding for Intersection Improvements:** In recognition of the fact that this project will add traffic to an already congested area with problematic intersections, the Applicant has agreed to contribute \$25,000 to the Town for the purpose of implementing recommendations within the peer review conducted and provided by Green International Affiliates, Inc. This funding shall be managed by the Town Engineer and/or Community Development Director for such improvements that are deemed necessary.

Prior to the Issuance of a Certificate of Occupancy:

- 1) **Architecture:** The building façade on each elevation (north, south, east, and west) shall be substantially as indicated on the approved architectural plans and elevations.
- 2) **Stormwater O&M Plan:** A post-construction Operations and Maintenance Plan for the stormwater system shall be provided to the Town Engineer.
- 3) **Traffic Signal Phasing and Optimization:** The Applicant shall engage with the Town’s Engineering Department to review and optimize traffic signal timing based on projected traffic volumes prior to the occupancy of the development. The traffic signal timing shall be reexamined 6-months after occupancy for further necessary adjustments.
- 4) **Property Management / Leasing Documents:** A copy of the finalized Property Management / Leasing Documents shall be submitted to the Community Development Director for review and approval, and shall contain the following language:
 - a. **Fire Safety:** Language ensuring fire safety by prohibiting gas/propane grills on balconies, etc.
 - b. **Delivery Vehicles & Times:** Language prohibiting commercial deliveries along Main Street and Washington Street, and prohibiting commercial deliveries to the commercial space between 10:00 PM and 5:00 AM.
 - c. **Management of Move-ins & Move-outs:** Language regarding management of move-ins and move-outs by the on-site property manager, specifically with regards to the size of moving vehicles allowed and the timing and use of the garage.
 - d. **Drainage System Maintenance:** Language that requires the property management company to adhere to the requirements of the Stormwater O&M Plan.
 - e. **Trash Removal:** Language detailing how trash and recycling will be managed on-site, including but not limited to schedule of pick-up days and times, and logistics for trash truck access to the site. Trash management for both the residential and retail uses shall be managed separately, as is practicable.
 - f. **Snow Removal:** Language that requires the property management company to adhere to the requirements and specifications of the Snow and Ice Removal Plan submitted.
 - g. **Site Lighting:** Language that commercial lighting (including signage) shall be programmed to shut off at the close of business each day.
 - h. **Conditions for Ongoing Maintenance after Occupancy:** Language that the property management company shall adhere to the “Conditions for Ongoing Maintenance after Occupancy” as are stated herein below.
- 5) **Rooftop Mechanicals:** All rooftop mechanicals shall be set back from building facades and appropriately screened from view.
- 6) **Pedestrian Improvements:** Improvements along Main Street, Washington Street and abutting rights-of-way, as deemed necessary or advantageous to the Town Engineer and Community Development Director, to adjacent crosswalks, signal heads, curbing and sidewalks, shall be installed at the Applicant’s expense in accordance with Town standards.
- 7) **Garage Striping:** Garage parking spaces shall be striped in accordance with the approved plans. Dimensions shall be measured from centerline to centerline. Striping of parking lanes,

and directional arrows, consistent with current Manual on Uniform Traffic Control Devices (MUTCD), shall be installed within the garage.

- 8) **I/I Fee:** The Applicant is subject to the required Inflow/Infiltration Fee as the proposed sewer flow usage will be greater than historical usage. The Fee is calculated as twice the flow times \$4.00.
- 9) **Street Trees:** The Applicant shall work with the Tree Warden to locate the street tree along Main Street to an appropriate location. Both the species and location shall be approved by the Town's Tree Warden.
- 10) **Commercial Tenant Space(s):** The approved plans and parking ratio are based on an intended retail, restaurant, or certain other commercial use of the commercial tenant space(s). No office or institutional use shall occupy the commercial tenant space(s) until the property owner or prospective tenant submits a plan to the CPDC demonstrating how commercial parking demand will be met and/or applies and receives a waiver from the CPDC for the parking requirement.
- 11) **Lighting:** All exterior building and site lighting shall comply with the dark sky initiatives (light shall shine down only) with the light source being fully shielded (with cutoff shields) so that little to no light or glare spills onto abutting properties. Spec sheets of proposed lighting fixtures shall be submitted to the Community Development Director for review and approval.

Conditions for Ongoing Maintenance after Occupancy:

- 1) **Parking Utilization Data:** The Applicant or Owner shall provide reports to the Community Development Director indicating utilization of the on-site parking and shall work with Town staff to evaluate impacts and make any necessary modifications to the parking space management system described above, including the provision of EV charging infrastructure, Zip Car spaces, etc. if evidenced to be desired by tenants.
- 2) **Energy Utilization Data:** The Applicant shall provide reports to the Community Development Director detailing the building's annual energy use and corresponding greenhouse gas emissions. A report shall be created utilizing the building's benchmark year and Owners shall report energy use for all metered uses to the Town.
- 3) **Signage:** Prior to installation of any building or tenant signage, a Sign Permit Application and/or Master Signage Plan shall be submitted for review and approval.
- 4) **As-Built Plans:** Two full size paper copies and electronic AutoCAD final As-Built plans showing the building footprint, drainage systems and utility connections shall be submitted to the Community Development Director and Town Engineer to ensure compliance with this decision and other applicable Town standards. The bond held for this requirement will be returned to the Applicant once this condition has been fulfilled.
- 5) **Landscaping:** Landscaping on-site shall be maintained in a healthy condition in perpetuity. In the event that landscaping is damaged during snow removal operations, the property owner shall replace such landscaping during the next growing season.
- 6) **Lighting:** All exterior building and site lighting shall comply with the dark sky initiatives (light shall shine down only) with the light source being fully shielded (with cutoff shields) so that little to no light or glare spills onto abutting properties. Any exterior lighting that is required for security purposes may be illuminated by photocells and is not required to be extinguished at the close of business. All site and building lighting for commercial purposes,

beyond what is needed for security purposes, shall be programmed to shut off at the close of business each day.

- 7) **Trash Removal:** All trash collection and disposal are the responsibility of the owner / property manager. The Applicant shall ensure daily that exterior areas of the site remain clear of debris, trash and any equipment used in connection with any commercial activities on site.

Plan Changes after Approval by the Commission:

Contemplated future changes to the plan approved herein shall be presented to the Community Development Director and the Building Inspector, or other relevant Town staff, for review prior to implementation of proposed changes.

10.5.13.1 Minor Plan Changes: After Plan Approval, an Applicant may apply to make minor changes in a Development Project involving minor utility or building orientation adjustments, or minor adjustments to parking or other site details that do not affect the overall build out or building envelope of the site, or provision of open space, number of housing units, or housing need or affordability features. Such minor changes must be submitted to the Commission on redlined prints of the approved plan, reflecting the proposed change, and on application forms provided by the Commission. The Commission may authorize such changes at any regularly scheduled meeting, without the need to hold a public hearing. The Commission shall set forth any decision to approve or deny such minor change by motion and written decision, and provide a copy to the Applicant for filing with the Town Clerk.

10.5.13.2 Major Plan Changes: Those changes deemed by the Commission to constitute a major change in a Development Project because of the nature of the change in relation to the prior approved plan, or because such change cannot be appropriately characterized as a minor change as described above, shall be processed by the Commission as a new application for Plan Approval pursuant to Section 10.5.

Appeal:

Any person aggrieved by this Decision of the CPDC may appeal to the appropriate court in accordance with the provisions of M.G.L. Ch. 40A Section 17, pursuant to M.G.L. Ch. 40R Section 11, within twenty (20) days after the date of filing of this Decision with the Town Clerk. Notice of any appeal with a copy of the complaint must also be filed with the Town Clerk within such twenty (20) days as provided in M.G.L. Ch. 40A Section 17.

This Decision and the relief, terms, restrictions and conditions contained herein shall run with the land and all subsequent owners shall benefit from and be bound by the relief, terms, restrictions and conditions contained herein.

Signed as to the accuracy of the vote as reflected in the minutes:

Andrew MacNichol, Senior Planner
Cc: Applicant, Town Clerk, DRT Staff, planning file

Date



GREEN INTERNATIONAL AFFILIATES, INC.

100 AMES POND DRIVE, SUITE 200 TEWKSBURY, MA 01876

T: (978) 923-0400 | WWW.GREENINTL.COM

August 18, 2022

Ms. Julie Mercier, AICP, LEED AP
Community Development Director
Town of Reading
16 Lowell Street
Reading, MA 01867

Subject: **Review of Responses to Comments
Proposed Mixed-Use Development
459 Main Street
Reading, Massachusetts**

Dear Ms. Mercier:

On behalf of the Town of Reading (the Town), Green International Affiliates, Inc. (Green) is submitting this letter of review of Regnante Sterio LLP's (RS) responses to the original comments submitted by Green to the Town on August 9, 2022 from our engineering peer review of the application package for the proposed mixed-use development at 459 Main Street. This review included an examination of the following documents submitted in support of the proposed project:

- Traffic Impact Assessment (TIA) prepared by The Engineering Corp (TEC), dated November 17, 2021;
- Technical Memorandum, prepared by TEC, dated March 3, 2022;
- Supplemental Traffic Impact Assessment, prepared by TEC, dated June 1, 2022, which outlines turning movements into and out of the site that present concerns for the Community Planning & Development Commission (CPDC);
- Civil Engineered Plan Set, prepared by Fodera Engineering, most recently dated June 1, 2022;
- Architectural Plan Set, prepared by Olinger Architects, most recently dated June 1, 2022;

What follows are the original comments submitted by Green, followed by the corresponding RS response in italicized text, followed by Green's latest comments in bold text

November 2021 (Updated March 2022) Transportation Impact Assessment and March 2022 Supplemental Technical Memorandum

1. Green's original comment: Stopping Sight Distance and Intersection Sight Distance were measured for the proposed egress driveway on Washington Street. It was found that the ASSHTO recommended minimum sight distances are available. Green concurs with this but notes that traffic queuing at the intersection of Washington Street and Main Street regularly blocks the sight distance of drivers wishing to turn left out of the driveway. We recommend that left turns out of the Washington Street driveway be restricted to alleviate this safety concern.

RS Response: The applicant is agreeable to a condition implementing the recommendation of right-turn only control from the Washington Street driveway.

Green Response: No further information is required for the purpose of the peer review. Please continue to coordinate with the Town for implementation of the proposed turn restriction.

2. Green's original comment: Trip distribution for the retail land use was based on existing vehicle travel patterns. Green concurs with this methodology and no further information is required.

RS Response: No response is called for.

Green Response: **No further information is required.**

3. Green's original comment: Trip distribution for the residential land use was based on 2009-2013 U.S. Census Journey-to-Work/Home data for the Town. Green recommends that the residential trip distribution be performed using the most recently available Census Bureau On-the-Map data. Based on a cursory review, the distribution for the most recently available year (2019) differs from what is shown in the trip distribution model.

RS Response: TEC acknowledges that more up-to-date information from US Census Bureau may be available. However, based upon TEC's review of this data, any change in distribution would not result in any significant change in traffic volumes to/from the site. The change in traffic volumes related to this distribution change is therefore negligible and would not materially affect the conclusions of the TIA.

Green Response: **It is understood that the trip distribution change would not materially affect the TIA's conclusions; however, based on feedback from the 8/8/2022 CPDC meeting, the Applicant should revise the trip distribution per the latest census data. Abutters and CPDC board members stated that even minor changes in vehicle volume should be acknowledged.**

4. Green's original comment: Based on capacity analysis results and Green's observations during our site visit, queues on both Main Street and Washington Street regularly extend past the proposed entrance and exit driveways. As mentioned in comment #1, we recommend restricting left turns out of the garage onto Washington Street. We concur that providing an entrance driveway on Main Street and an exit driveway on Washington results in the optimal site circulation pattern.

RS Response: As noted, the applicant is agreeable to a condition implementing the recommendation of right-turn only control from the Washington Street driveway.

Green Response: **No further information is required for the purpose of the peer review. Please continue to coordinate with the Town for implementation of the proposed turn restriction.**

5. Green's original comment: The TIA states that the Applicant will work with the Town to optimize traffic signal timings post-occupancy if necessary. We recommend that traffic signal timings be optimized based on projected traffic volumes prior to the opening of the development and re-examined 6 months after opening for any further necessary adjustments.

RS Response: Understood and agreed-to.

Green Response: **No further information is required.**

6. Green's original comment: The TIA states that although 4 parking spaces are provided in the parking garage for the commercial use, the commercial space within the site is exempt from off-street parking requirements since it is within 300 feet of public off-street parking along Woburn Street; however, the site is located over 1,500 feet from Woburn Street. Based on field observations, off-site parking seems to be limited and difficult to locate in the project area. Please clarify which off-street parking lots are expected to be utilized.

RS Response: Public parking is available in multiple locations in the vicinity of the project site including twelve on-street parking along Main Street north of the site, six head-in parking spaces along Ash Street immediately opposite the site, and six on-street parking along Ash Street. Additional on-street parking is available at multiple other locations within 1,500-feet of the site as part of the Downtown parking network.

Green Response: Per discussion at the 8/8/2022 CPDC meeting, on-site parking is not required for retail nor residential use since the project is located in the Downtown Smart Growth District. Because some of the spaces in the parking garage are intended for the commercial land use, the Applicant should provide signage to indicate that commercial parking is available on-site.

7. Green's original comment: Fifteen parking spaces are required for the twelve residential units use per Town zoning bylaws. The proposed parking garage does comply with this requirement as it provides fifteen parking spaces for the residential use. One accessible parking space is required; the proposed parking garage provides two accessible spaces. The TIA states that 19 parking spaces are provided (15 for residential and 4 for commercial), but the plans show 20 spaces. Please clarify the total number of proposed parking spaces.

RS Response: Twenty (20) total spaces are proposed. The TIA's reference to 19 spaces is based on an earlier version of the project plans, which have since been amended to provide one additional parking space in response to project feedback.

Green Response: No further information is required.

June 2022 Civil Engineering Plan Set and June 2022 Architectural Plan Set

1. Green's original comment: Green reviewed the passenger car turning movements shown in the architectural plan set and found them to be acceptable. The Applicant should provide a figure showing emergency vehicle turning movements into and out of the parking garage as well as circulation through the garage for review.

RS Response: Please see attached diagram.

Green Response: The attached diagram does not show emergency vehicle turning movements out of the parking garage. Please provide turning movements out of the garage and the template that was used for the ambulance.

2. Green's original comment: Green reviewed the proposed loading zone location and believe that there is sufficient width for delivery vehicles to load/unload while allowing other vehicles to bypass. Please provide turning movements showing that parking spaces 18, 19, and 20 can be accessed while the loading area is occupied.

RS Response: It is anticipated that vehicles using these spaces would likely reverse into these when the loading zone is occupied. Please see attached diagram.

Green Response: We have reviewed the turning movement diagram and request that the Applicant provide the vehicle template that was utilized.

3. Green's original comment: The TIA states that the loading zone will be delineated with pavement markings. Please show these proposed markings on the plan. Additionally, please ensure that adequate lighting will be provided at the loading zone such that it is visible to vehicles turning into the garage.

RS Response: This detail will be added to the final civil plan, as requested.

Green Response: Green will review the final plan when available.

Recommended Off-Site Mitigation Measures

1. Green's original comment: Because most parking for commercial land uses is expected to be off-site and a portion of all trips are expected to be via transit, the Applicant should commit to enhancing pedestrian accommodations at the intersection of Main Street and Washington Street. As the Applicant plans to upgrade the curb ramp at the northeast corner of the intersection, ADA requires that the curb ramps on the other three corners be upgraded as well. Further enhancements could include upgrading pedestrian signal equipment to meet current ADA standards We recommend that existing crosswalks be replaced with textured crosswalks similar to those at the intersections along Main Street north of Washington Street to provide better contrast and a higher level of safety.

RS Response: The project will comply with any/all applicable ADA requirements. The applicant looks forward to a further discussion of this issue with GIA's reviewer.

Green Response: Green acknowledges that all ADA requirements will be met by the Applicant. We are available for continued discussion of additional upgrades with the Applicant and the Town of Reading. It is noted that any traffic signal upgrades will need to be coordinated with and approved by the Parking, Traffic, Transportation Task Force (PTTTF).

2. Green's original comment: All vehicular traffic entering and exiting the site is expected to utilize the signalized intersection of Main Street and Washington Street. The Applicant should commit to funding safety improvements at the intersection such as upgrading all signal heads to have backplates and retroreflective borders. We also recommend evaluating the option of revising traffic signal phasing such that the southbound protected left-turn movement and northbound protected left-turn movement are switched. It appears that this would allow vehicles to safely make a left turn into the parking garage after the northbound and southbound through movements if they were not able to find a sufficient gap in traffic.

RS Response: The applicant looks forward to a further discussion of this issue with GIA's reviewer.

Green Response: We are available for continued discussion of additional upgrades with the Applicant and the Town of Reading. It is noted that any traffic signal traffic signal upgrades will need to be coordinated with and approved by the PTTTF.

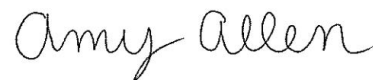
3. Green's original comment: As mentioned previously, traffic signal timings should be optimized before the development opens and re-examined 6 months later.

RS Response: Understood and agreed-to. No further response needed.

Green Response: **No further information is required.**

If either the Town staff or the Applicant's engineer would like to discuss any of these comments further, please feel free to contact me at 978-843-5281.

Sincerely,
Green International Affiliates, Inc.



Amy Allen, P.E., PTOE
Transportation Planning Group

cc: W. Wong, Green

Y:\Shared\Engineering\Projects\2022\22095\DOCS\RPTS\459 Main Street Peer Review_responses.docx



4 G000_COVER PLAN
1" = 40'-0"



3 G000_SW AXO



2 G000_NW AXO



459 MAIN STREET REDEVELOPMENT

459 MAIN STREET
READING, MA 01867

DEVELOPER: GC FODORA

ZONING: 40R - DSGD

ISSUED:

1. 11/17/2021 READING STAFF REV.
2. 12/01/2021 ZONING APPLICATION
3. 01/18/2022 ZONING UPDATE
4. 02/07/2022 PLANNING UPDATE
5. 03/07/2022 PLANNING UPDATE
6. 04/05/2022 PLANNING UPDATE
7. **09/07/2022 PLANNING UPDATE**

Sheet Number	Sheet Name	Sheet Issue Date	Current Revision Date
G000	COVER	12/01/2021	04/05/2022
G001	PROJECT INFORMATION	12/01/2021	03/07/2022
G002	ZONING GUIDELINES	12/01/2021	05/31/2022
G003	SHADOW STUDY	12/01/2021	01/18/2022
G004	PROJECT VIEWS	12/01/2021	04/05/2022
G005	GROUND LEVEL AXO	12/01/2021	05/31/2022
G006	PROJECT MATERIALS BOARD	12/01/2021	01/18/2022
G007	CONTEXT ELEVATIONS	12/01/2021	01/18/2022
C-1	EXISTING CONDITIONS SURVEY	12/01/2021	
C-2	PROPOSED SITE PLAN	12/01/2021	
C-3	PROPOSED SITEPLAN	12/01/2021	
C-4	DETAILS SHEET	12/01/2021	
C-5	DETAILS SHEET	12/01/2021	
C-6	VEHICLE TRACKING	09/07/22	
A001	SITE PLAN	12/01/2021	03/07/2022
A002	EXTERIOR AREA DETAIL PLANS	12/01/2021	05/31/2022
A100	BASEMENT PLAN	02/01/22	03/07/2022
A101	LEVEL 01 PLAN	12/01/2021	09/07/2022
A102	LEVEL 02 PLAN	12/01/2021	05/31/2022
A103	LEVEL 03 PLAN	12/01/2021	01/18/2022
A104	LEVEL 04 PLAN	12/01/2021	01/18/2022
A105	ROOF PLAN	12/01/2021	01/18/2022
A300	COMPOSITE ELEVATIONS	12/01/2021	05/31/2022
A310	WEST BUILDING ELEVATION	12/01/2021	03/07/2022
A312	SOUTH BUILDING ELEVATION	12/01/2021	05/31/2022
A313	EAST BUILDING ELEVATION	12/01/2021	05/31/2022
A314	NORTH BUILDING ELEVATION	12/01/2021	04/05/2022
A315	CORNER & MISC. ELEVATIONS	12/01/2021	01/18/2022
A400	BUILDING SECTIONS	12/01/2021	04/05/2022
A410	WALL SECTIONS 1	12/01/2021	05/31/2022
A411	WALL SECTIONS 2	12/01/2021	04/05/2022
A500	STAIR 01	12/01/2021	01/18/2022
A501	STAIR 02	12/01/2021	01/18/2022

PROJECT NAME
459 MAIN STREET REDEVELOPMENT

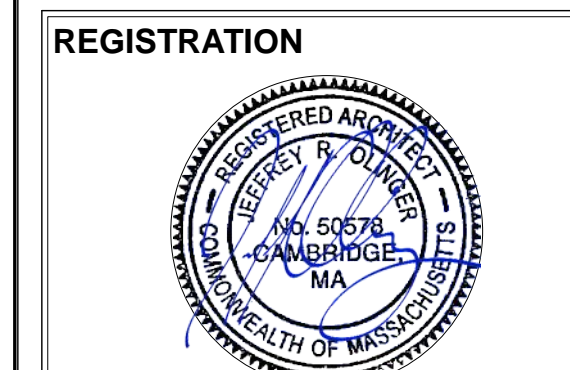
PROJECT ADDRESS:
459 MAIN STREET, READING, MA 01867

APPLICANT
Giuseppe C. Fodera
GCFodera Contracting Company
28 Harbor Street
Danvers, MA 01923

ARCHITECT
Olinger Architects

77 PLEASANT STREET
CAMBRIDGE, MA 02139
(857) 777-6108
INFO@OLINGER.IO

CIVIL ENGINEER
FODERA
ENGINEERING
28 Harbor St., Suite 204
Danvers, MA 01923
Office: (617) 992-8492



ZONING REVIEW

Project number 2122
Date 01/18/2022
Drawn by HP
Checked by JRO
Scale 1" = 40'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022
3	ZONING UPDATE	04/05/2022

COVER

G000

459 MAIN STREET

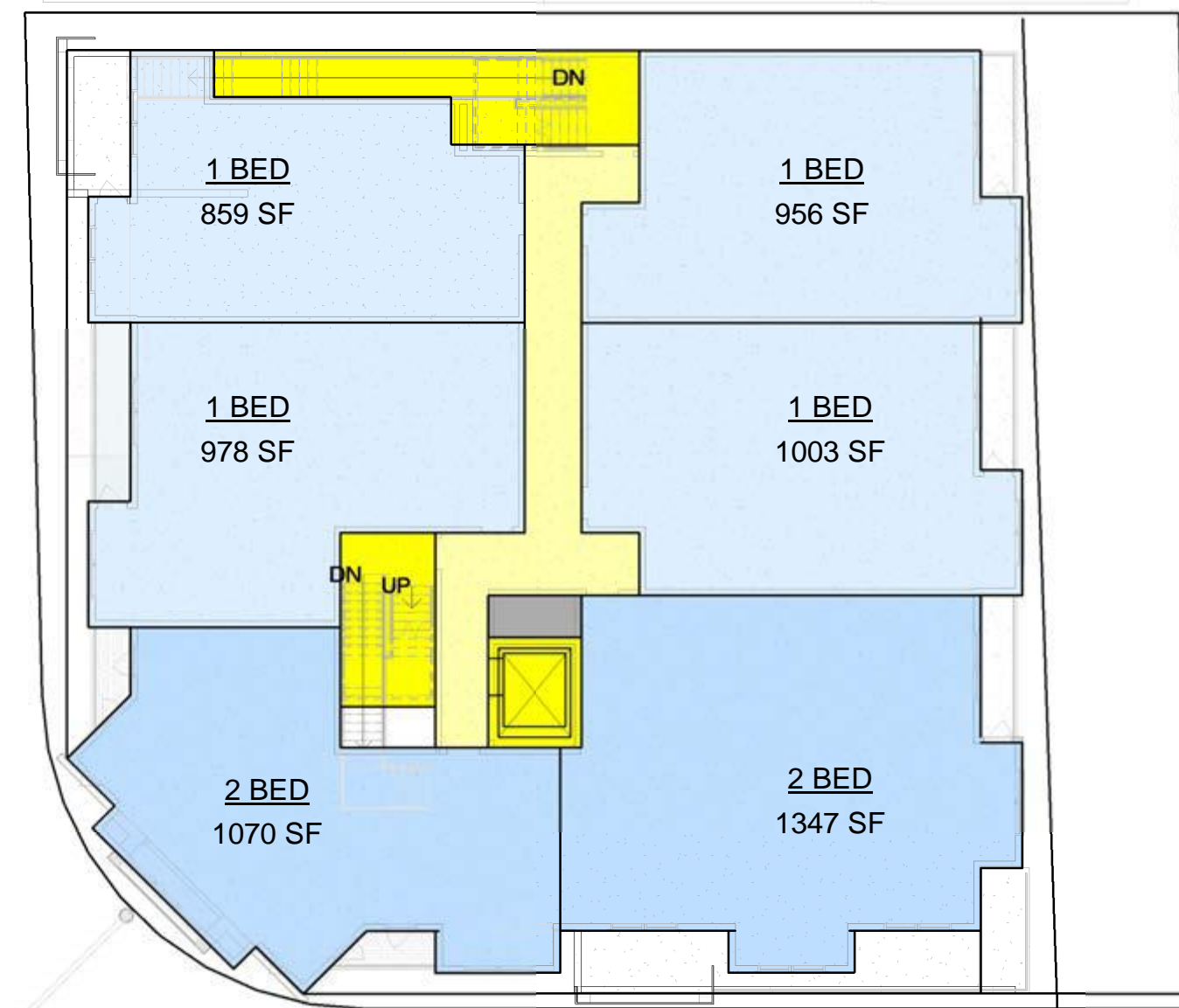
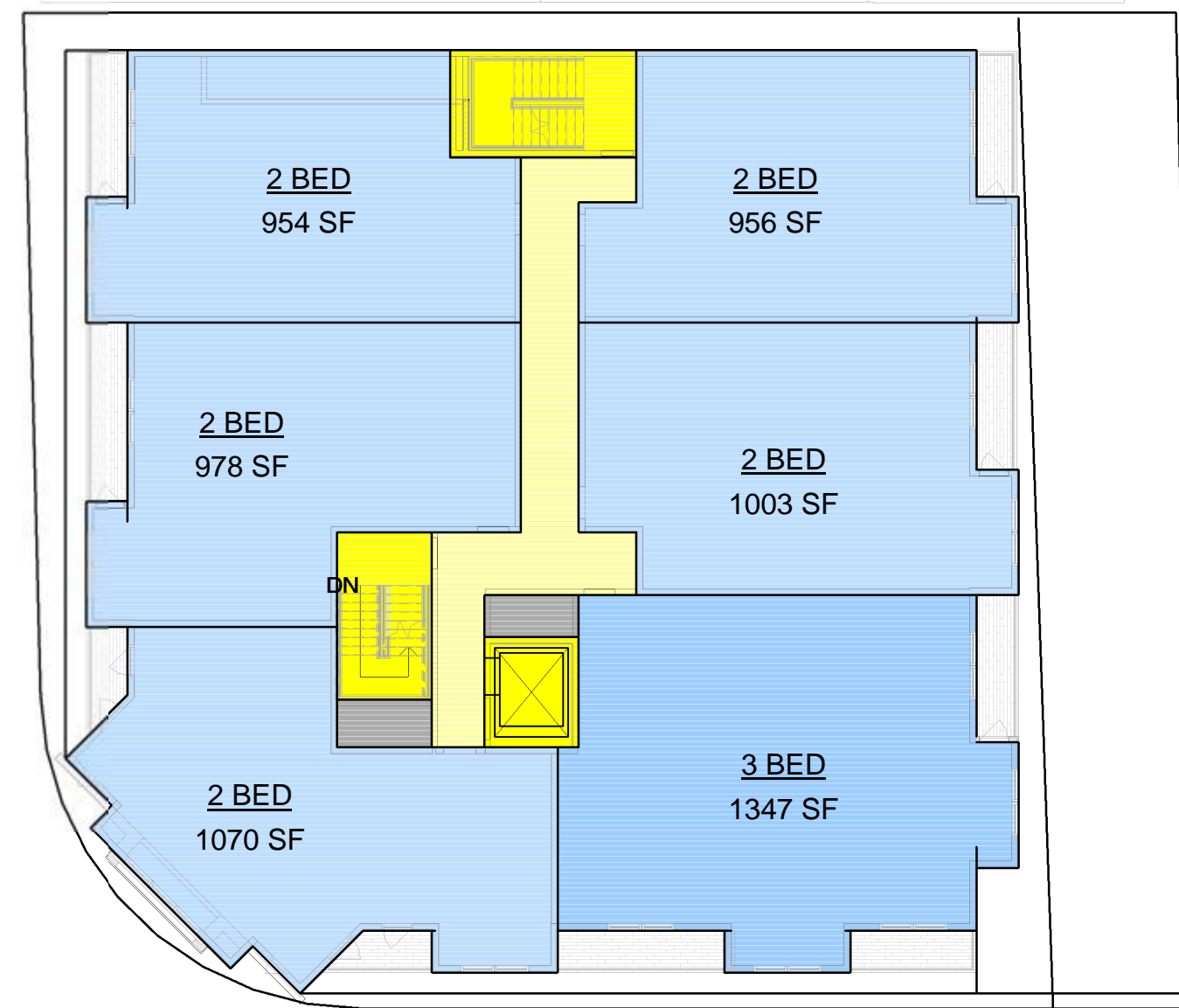
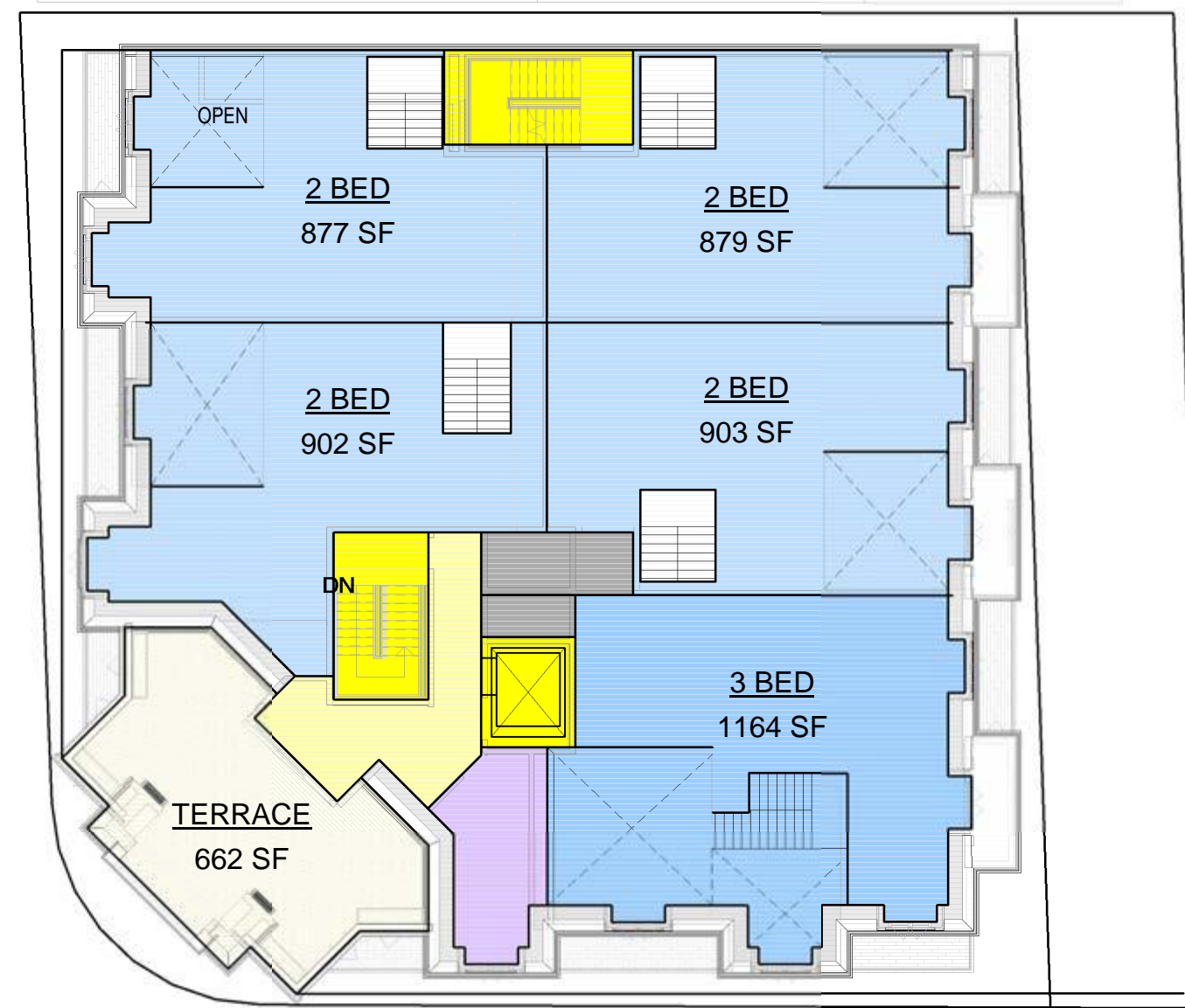
PROJECT NAME
459 MAIN STREET REDEVELOPMENT

PROJECT ADDRESS:
 459 MAIN STREET, READING, MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



4 G001_LEVEL 04
 1/16" = 1'-0"

GFA DETAIL	
Name	Area
ELEV.	95 SF
STAIR 2	182 SF
STAIR 2	144 SF
LEVEL 04	
2 BED	877 SF
2 BED	879 SF
2 BED	903 SF
2 BED	902 SF
3 BED	1164 SF
AMMENITY	172 SF
EGRESS	242 SF
ELEV.	95 SF
STAIR 1	162 SF
STAIR 2	144 SF
ROOF	
ELEV.	118 SF
STAIR 1	181 SF
Grand total: 41	23985 SF

3 G001_LEVEL 03
 1/16" = 1'-0"

GFA DETAIL	
Name	Area
ELEV.	95 SF
STAIR 2	263 SF
STAIR 2	150 SF
LEVEL 03	
2 BED	954 SF
2 BED	956 SF
2 BED	1003 SF
2 BED	1070 SF
2 BED	978 SF
3 BED	1347 SF
EGRESS	410 SF

2 G001_LEVEL 02
 1/16" = 1'-0"

GFA DETAIL	
Name	Area
RETAIL	1455 SF
STAIR 1	95 SF
STORAGE	384 SF
LEVEL 02	
1 BED	859 SF
1 BED	956 SF
1 BED	1003 SF
1 BED	978 SF
2 BED	1347 SF
2 BED	1070 SF
EGRESS	423 SF

1 G001_LEVEL 01
 1/16" = 1'-0"

GFA DETAIL	
Name	Area
LEVEL 0B	
COMMON AREA	1378 SF
LEVEL 01	
ELEV.	95 SF
FAAP	32 SF
LOBBY	282 SF
LOBBY	109 SF
MAIL	38 SF

GFA APT RENTAL	
Name	Area
LEVEL 02	
1 BED	859 SF
1 BED	956 SF
1 BED	1003 SF
1 BED	978 SF
2 BED	1347 SF
2 BED	1070 SF
LEVEL 03	
2 BED	954 SF
2 BED	956 SF
2 BED	1003 SF
2 BED	1070 SF
2 BED	978 SF
3 BED	1347 SF
LEVEL 04	
2 BED	877 SF
2 BED	879 SF
2 BED	903 SF
2 BED	902 SF
3 BED	1164 SF
Grand total: 17	17246 SF

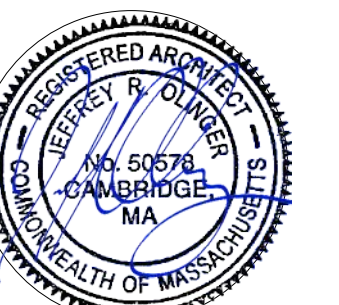
PROJECT GFA:

21,558 SF TOTAL GFA (W/O UTILITY)
 16,208 SF AREA LEVELS 2-4
 19,082 SF APT RENTAL GFA
 ~85% EFFICIENCY (W/O L1 & RF)

GFA FLOOR LEVEL AREA

Level	Area
LEVEL 0B	1378 SF
LEVEL 01	2488 SF
LEVEL 02	7143 SF
LEVEL 03	7137 SF
LEVEL 04	5540 SF
ROOF	299 SF
Grand total: 41	23985 SF

REGISTRATION



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	1/16" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022

PROJECT INFORMATION

G001

459 MAIN STREET

DSGD Design Guidelines

1. Introduction

The proposed development at 459 Main Street in Reading, MA is a Mixed-Use Transit Oriented Development that conforms with the Town's Downtown Smart Growth District Design Guidelines.

2. Purpose

This sheet is intended to assist with Project review by the Town. We have replicated the Town's Guidelines in outline form so that we may clearly communicate how this project satisfies the Town's requirements.

3. Applicability

The proposed development at 459 Main Street is located at the north-eastern boundary of the Town's DSGD Bus-B District and is abutting the Town's Single Family S-15 District, which makes the Town's DSGD Guidelines applicable to this parcel

4. Definitions

Definitions in ZBL Section 10.5.2 apply to these Design Standards and Guidelines. Where referenced, the Primary Commercial Streets in the DSGD shall be Main Street, Haven Street and High Street.

5. Application Requirements

Application for development within the District shall comply with the application requirements set forth in the Downtown Smart Growth District Bylaw (Reading Zoning Bylaw Section 10.5) and shall include all information noted below:

- a. Completed application form and application fee. (SEE OWNER RECEIPT DATED 11/22/2021)
- b. Development narrative including all uses, breakdown of square footage for each use, number of housing units and zoning summary. (SEE FIGURE 5.B/G-003)
- c. Locus map with lot identification. (SEE FIGURE 5.C/G-003)
- d. Existing site plan, including all existing structures, trees, topography and easements, prepared by a licensed surveyor. (SEE FIGURE 5.D/G-003)
- e. Certified list of abutters signed by the Town Assessor, including two sets of stamped and addressed envelopes to all abutters. (SEE OWNER RECEIPT DATED 11/22/2021)
- f. Photos of adjacent properties and other properties impacted by the development project. (SEE FIGURE 5.F/G-003)
- g. Proposed site plans, indicating project boundaries, building footprints, on-site and remote parking areas (where applicable), drainage and topography. All plans shall be prepared and stamped by a professional engineer. (SEE SHEET C-001_SITE PLAN)
- h. Proposed architectural plans, including floor plans for all levels including the roof, and building elevations for all sides including courtyards and interior lot elevations. All plans shall be prepared and stamped by a registered architect. (SEE SHEETS A-101 - A-104, A-300 - A-304)
- i. Landscape plan showing existing and proposed landscaping. (SEE FIGURE 5.D & SHEET G-004)
- j. Massing perspective sketches or renderings illustrating the key elements of the development proposal within its context. (SEE SHEET G-002)
- k. Proposed exterior lighting plan with photometric information. (SEE SHEET C-002_SITE LIGHTING PLAN)
- l. Proposed stormwater management plan with rainwater calculations, and O&M Plan. (SEE SHEET C-003_STORM WATER MANAGEMENT PLAN)
- m. Parking Plan including utilization of municipal lots, on-street parking, and shared parking arrangements, detailing on- and/or off-site accommodations to address the parking needs for proposed commercial spaces and employees. (SEE SHEET C-001_SITE PLAN)
- n. Traffic Study that accounts for all recently approved commercial, mixed-use and multi-family housing projects in and within a half-mile radius of the Downtown Smart Growth District. (SEE REPORT DATED 07/02/2021)
- o. Shadow Studies, if required pursuant to Section 10.5.5 herein. (SEE SHEET G-003_SHADOW STUDY)
- p. Sample boards and color palettes for all exterior building materials. (SEE SHEET G-006_PROJECT MATERIALS BOARD)
- q. Evidence that the Development Project complies with the cost and eligibility requirements of Reading Zoning Bylaw Section 10.5.10. (SEE OWNER ESTIMATE DATED 07/02/2021)
- r. Development Project plans that demonstrate compliance with the design and construction standards of Reading Zoning Bylaw Section 10.5.10.3. (SEE OWNER DEVELOPEMNT PLAN)
- s. A form of the Affordable Housing Restriction that satisfies the requirements of Reading Zoning Bylaw Section 10.5.10.5. (SEE OWNER DEVELOPEMNT PLAN)

FIGURE 5.B

DEVELOPMENT NARRATIVE

Approximate Unit Mix

- (12) Total number of residential units
 - (4) 1-Bedroom
 - (7) 2-Bedroom
 - (1) 3-Bedroom
- (1) Total number of commercial units
 - (1) Major Corner Retail Unit

Improved Public Realm Experience

- Enliven a currently one-dimensional parcel with a vibrant mix of uses designed to activate parcel throughout day and evening hours.
- New building will improve the view from mainstreet by replacing an aging tire repair station with a new development that better conforms with the DSGD objectives.
 - Opportunity for high value public space activated by retail uses
 - Presents a welcoming 'front porch' for visitors arriving to Reading's downtown along Main Street
- Remove 1 existing curb cuts to create a passenger loading space on Main Street

Parking

- (20) Garage parking spaces
- (15) Residential spaces = 12 x 1.25
- (4) Compact = 26.66% < 30%
- (1.66) Garage parking ratio surpasses required (1.25) Parking ratio

Features for Residents

- "Common" building roof deck
- All units have balconies
- ADA adaptable units
- Garage parking
- In-unit washer/dryer
- Sustainable building construction and unit features

Zoning Considerations

- 40R designation based upon mixed-use criteria
- Design FAR of 2.18 vs Zoned FAR of 2.8 (21,919 SF / 10,019 SF)
- **PARKING IS UNENCLOSED AND NOT INCLUDED IN FAR**
- Design parking of 1.25x vs zoned parking of 1.25x
- Design unit density exceeds 20 units/acre zoned density

*Includes unenclosed parking area. Inclusive of parking 1.68 of 1.72.

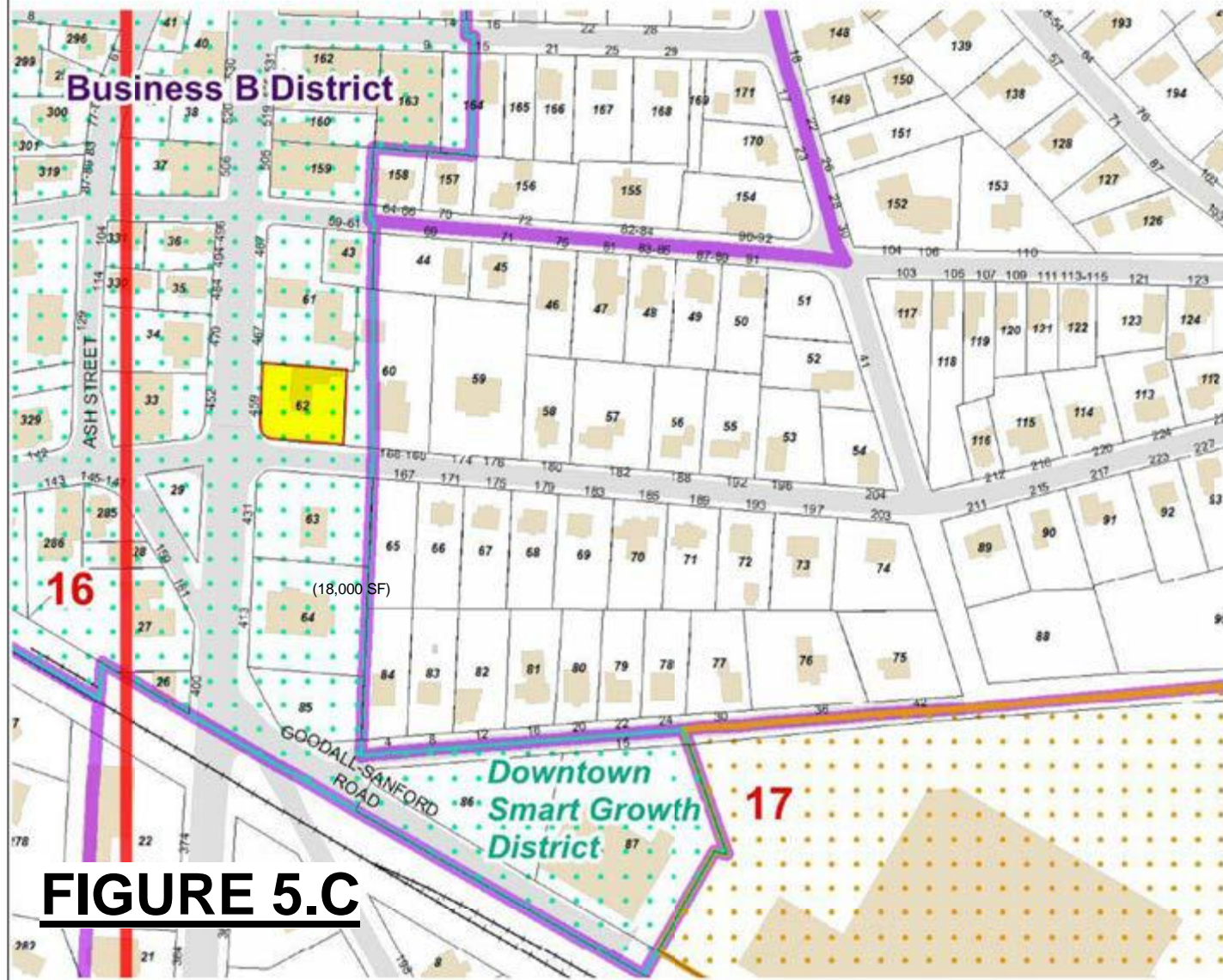


FIGURE 5.C

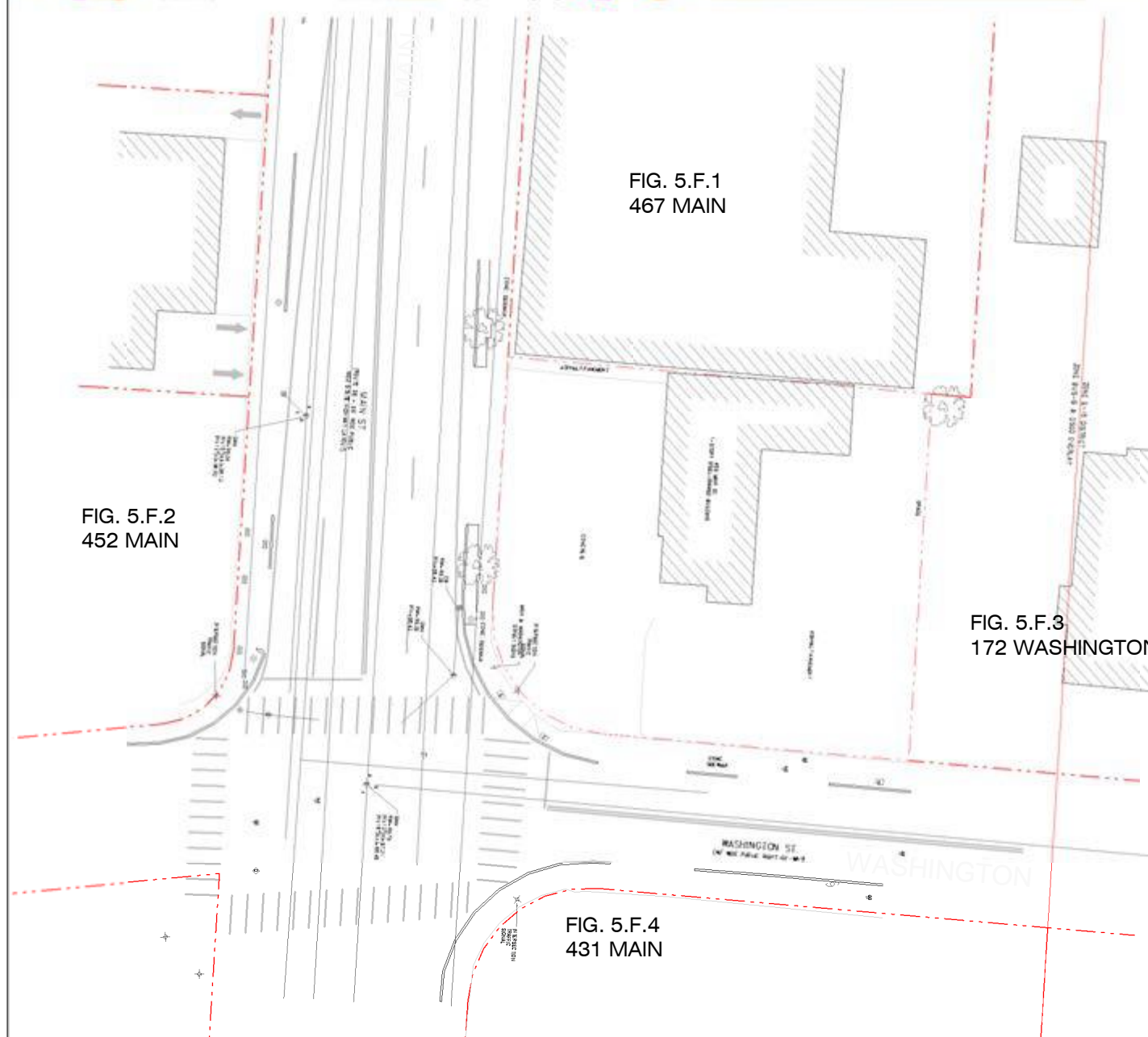


FIGURE 5.D

SCALE: 1" = 40'

FIGURE 5.F

ABUTTER PHOTOGRAPHS



FIGURE 5.O

SHADOW STUDY

SEE SHEET G-003 FOR DETAILED SHADOW STUDY



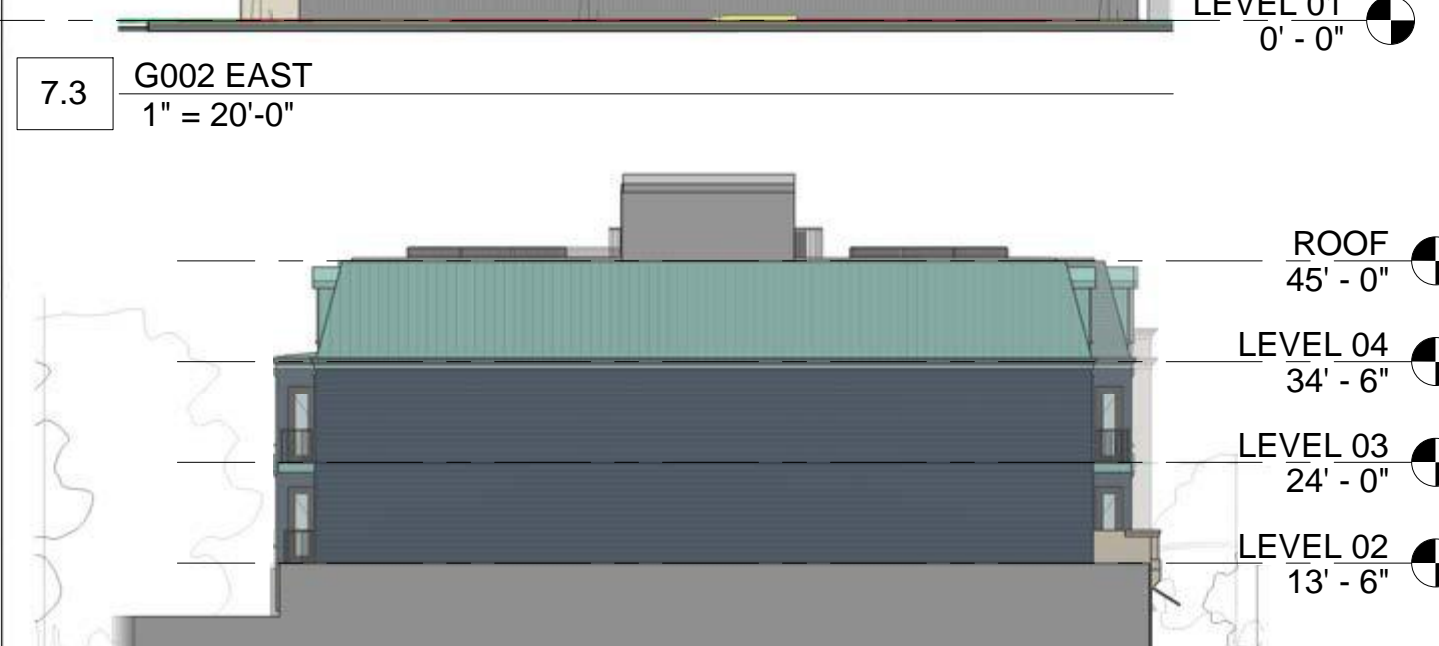
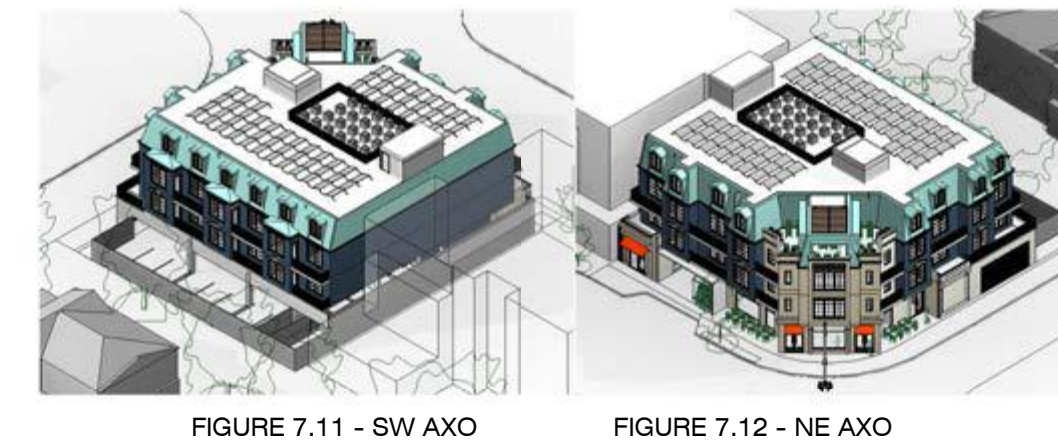
6. Guiding Principles

- 6.1 Neighborhood Character & Historic Context
This Project seeks to compliment and enhance the Town's Historic Context and Neighborhood Character
- 6.2 Mixed Use Development
This Project will provide over 1,500 sf of retail space in addition to 12 residential living units
- 6.3 Human Scale Design to Enliven Downtown
This Project seeks to activate the street scape at the corner of Main & Washington, and at various locations along the buildings's frontage to enhance the pedestrian experience
- 6.4 Connectivity and Relationship to Surrounding Neighborhoods
This Project seeks to create a continuous streetscape that marks a Start to the Main Street commercial corridor
- 6.5 Streetscape Character and Landscape Elements
This Project seeks to maximize the benefits of our our podium design, with periodic planters located at the second floor to soften the building's presence at the street
- 6.6 Smart Growth and Sustainable Development
This Project will utilize Low Impact Development Techniques where possible, and energy efficient sustainable design strategies capable of meeting Energy Star V3.1 standards

FIGURE 7

BUILDING DEIGN

- 7.1 Massing (SEE FIGS 7.11 & 7.12)
- 7.2 Appearance (SEE FIGS 7.21 - 7.25)
- 7.3 Entrines (SEE FIGS 7.21 - 7.25)
- 7.4 Fenestration (SEE FIGS 7.21 - 7.25)
- 7.5 Materials (SEE FIGS 7.21 - 7.25)



8. Site Design Standards

- 8.1 Sidewalks (SEE C-001 & ASSOCIATED DETAILS) (SEE G-004 FOR READING DSGD PEDESTRIAN RETAIL PLAN)
- 8.2 Driveways and Parking (SEE C-001 & ASSOCIATED DETAILS)
- 8.3 Landscaping (SEE A-101 STREET LEVEL PLAN)
- 8.4 Lighting (SEE C-002 & ASSOCIATED DETAILS)
- 8.5 Utility Areas and Utilities (SEE C-001 & ASSOCIATED DETAILS)
- 8.6 Drainage and Storm Water Management (SEE C-003 & ASSOCIATED DETAILS)
9. Signage Design Standards (SEE FIGURES 7.21 - 7.25) (SEE OWNER DEVELOPEMNT PLAN FOR ADDITIONAL INFORMATION)
10. Additional Considerations for District Edges and Transitional Areas (SEE SETBACK AT 4TH FLOOR AT LOT LINE ABUTTING S-15 ZONING DISTRICT REFERENCE FIGURES (7.21 - 7.25))

PROJECT NAME

459 MAIN STREET REDEVELOPMENT

PROJECT ADDRESS:

459 MAIN STREET, READING, MA 01867

APPLICANT

Giuseppe C. Fodera
GCFodera Contracting Company
28 Harbor Street
Danvers, MA 01923

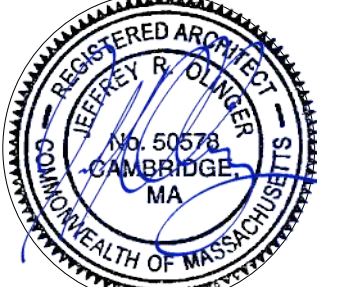
ARCHITECT

Olinger Architects

77 PLEASANT STREET
CAMBRIDGE, MA 02139
(857) 777-6108
INFO@OLINGER.IO

CIVIL ENGINEER
FODERA
ENGINEERING
28 Harbor St., Suite 204
Danvers, MA 01923
Office: (617) 952-8492

REGISTRATION



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	As indicated

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
3	ZONING UPDATE	04/05/2022
4	ZONING UPDATE	05/31/2022

ZONING GUIDELINES

G002

459 MAIN STREET

PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

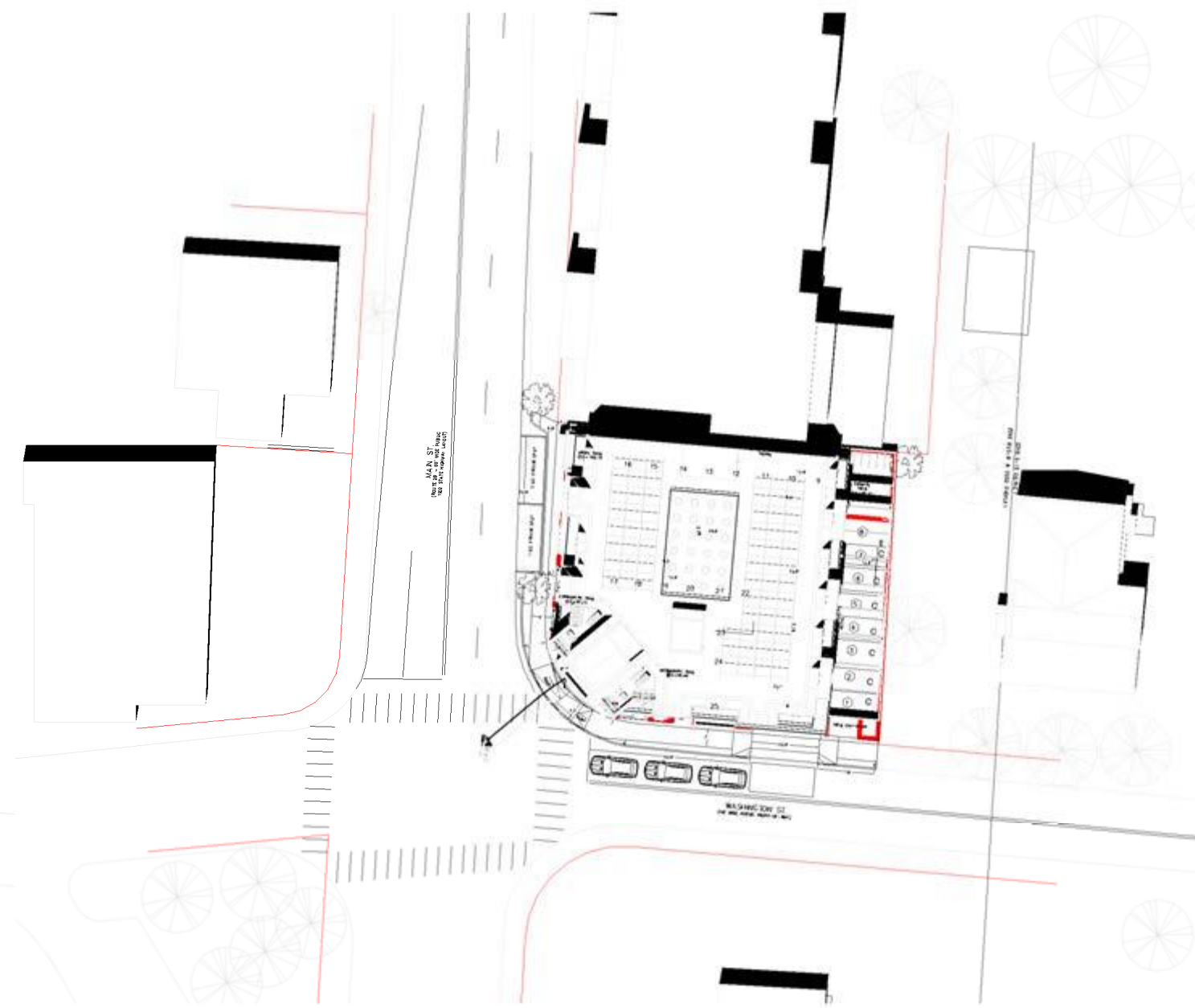
APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

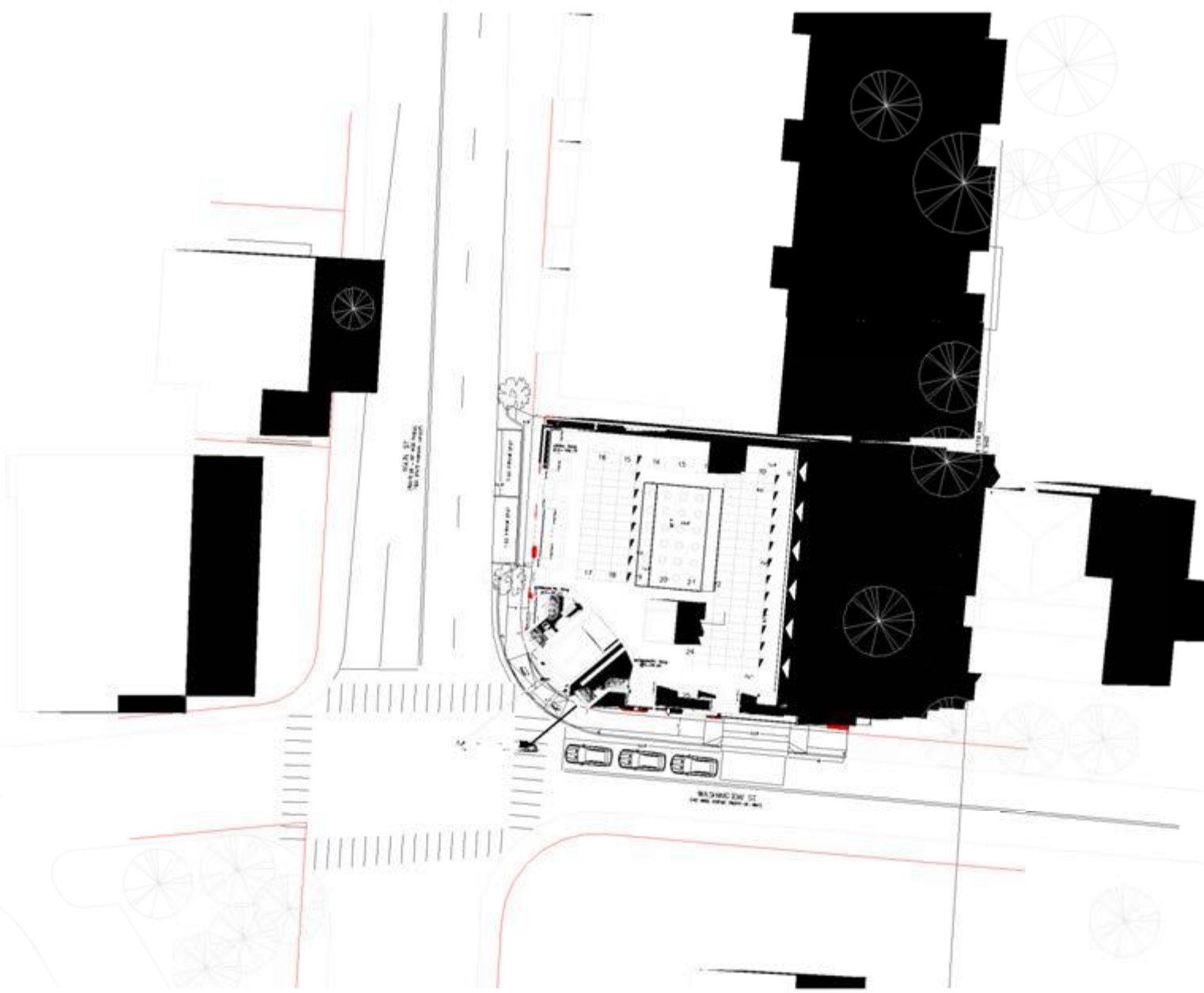
CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



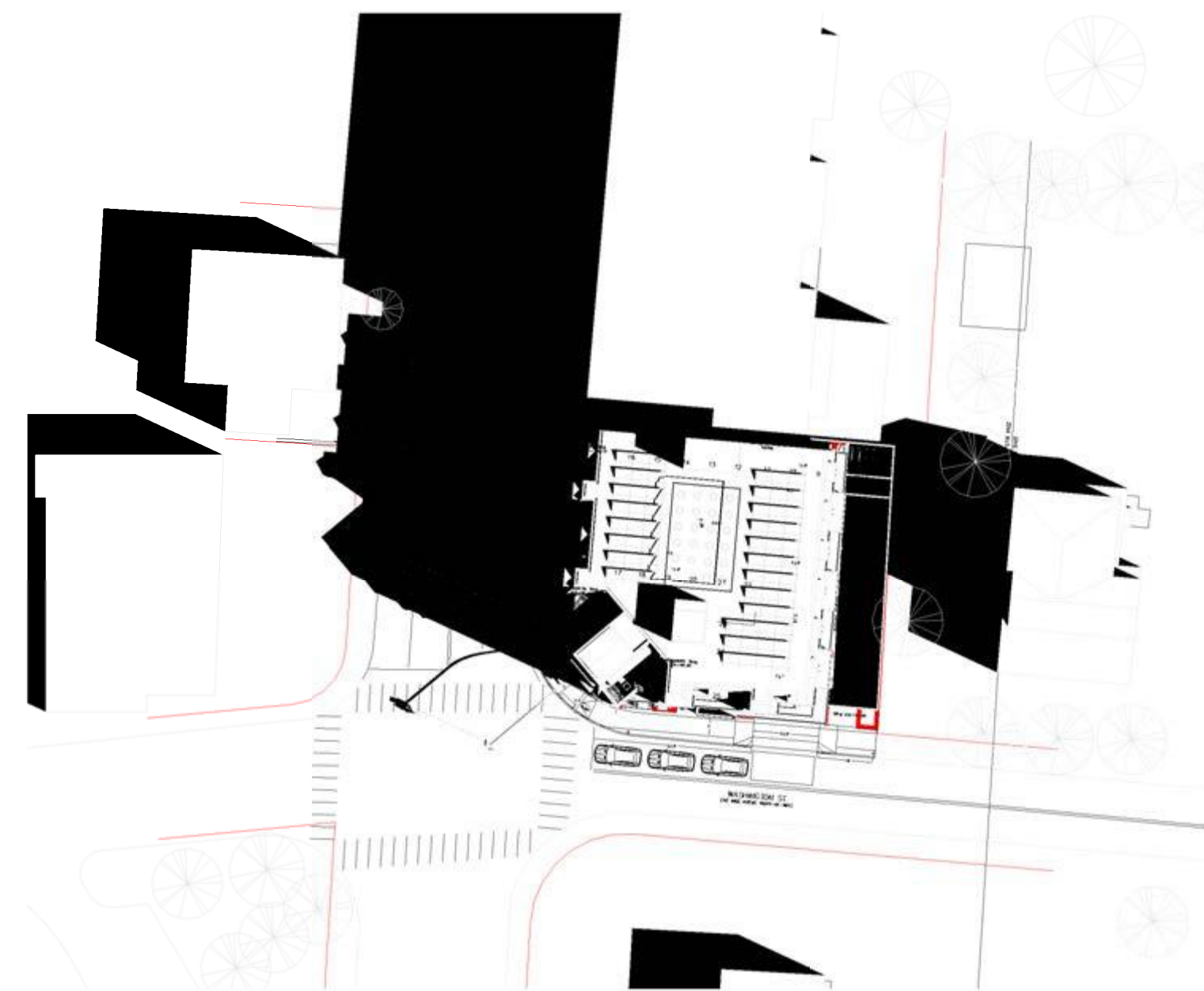
1 G002_SHADOW STUDY_06.20.0800
 1" = 50'-0"



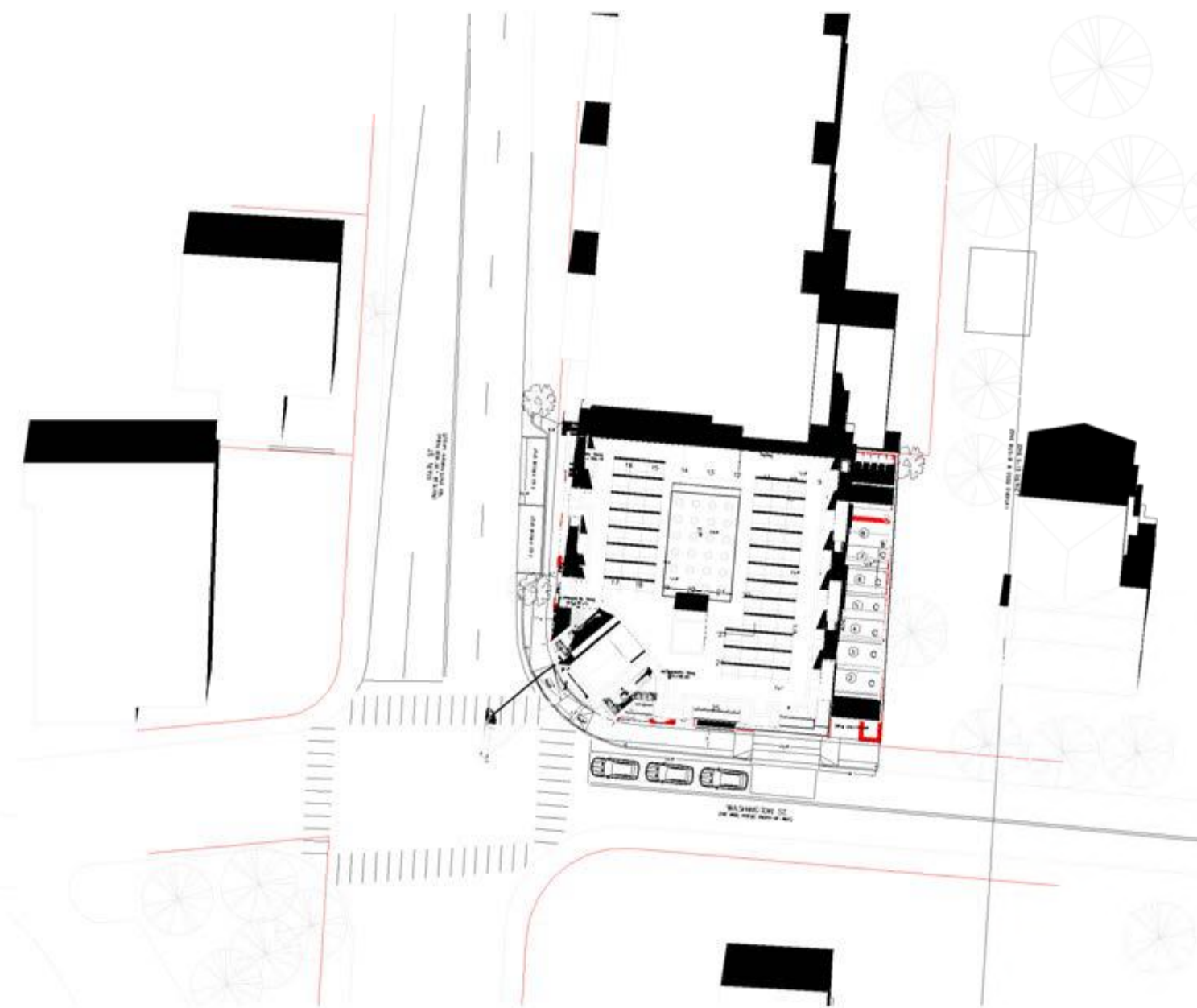
2 G002_SHADOW STUDY_06.20.1200
 1" = 50'-0"



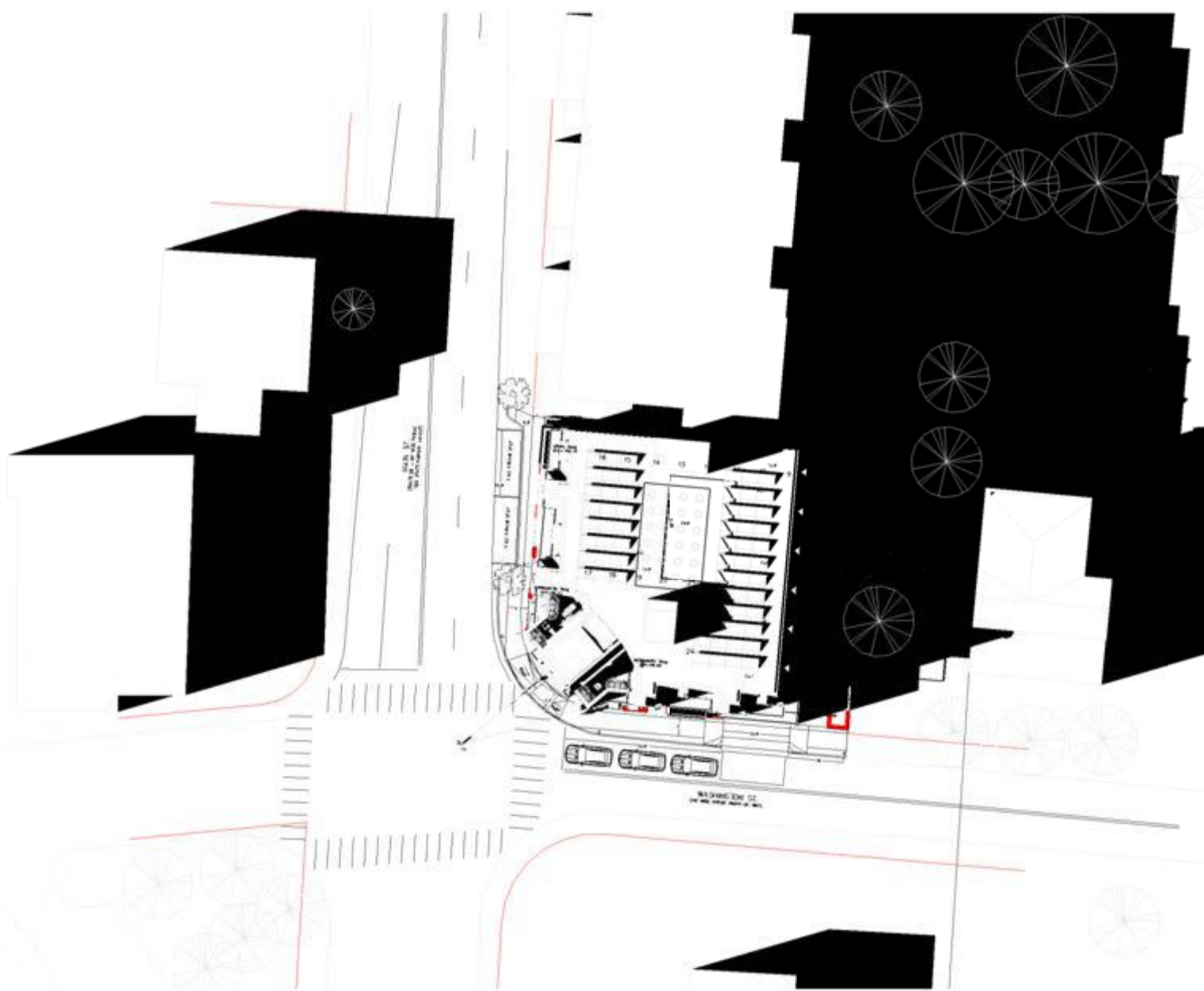
3 G002_SHADOW STUDY_06.20.1600
 1" = 50'-0"



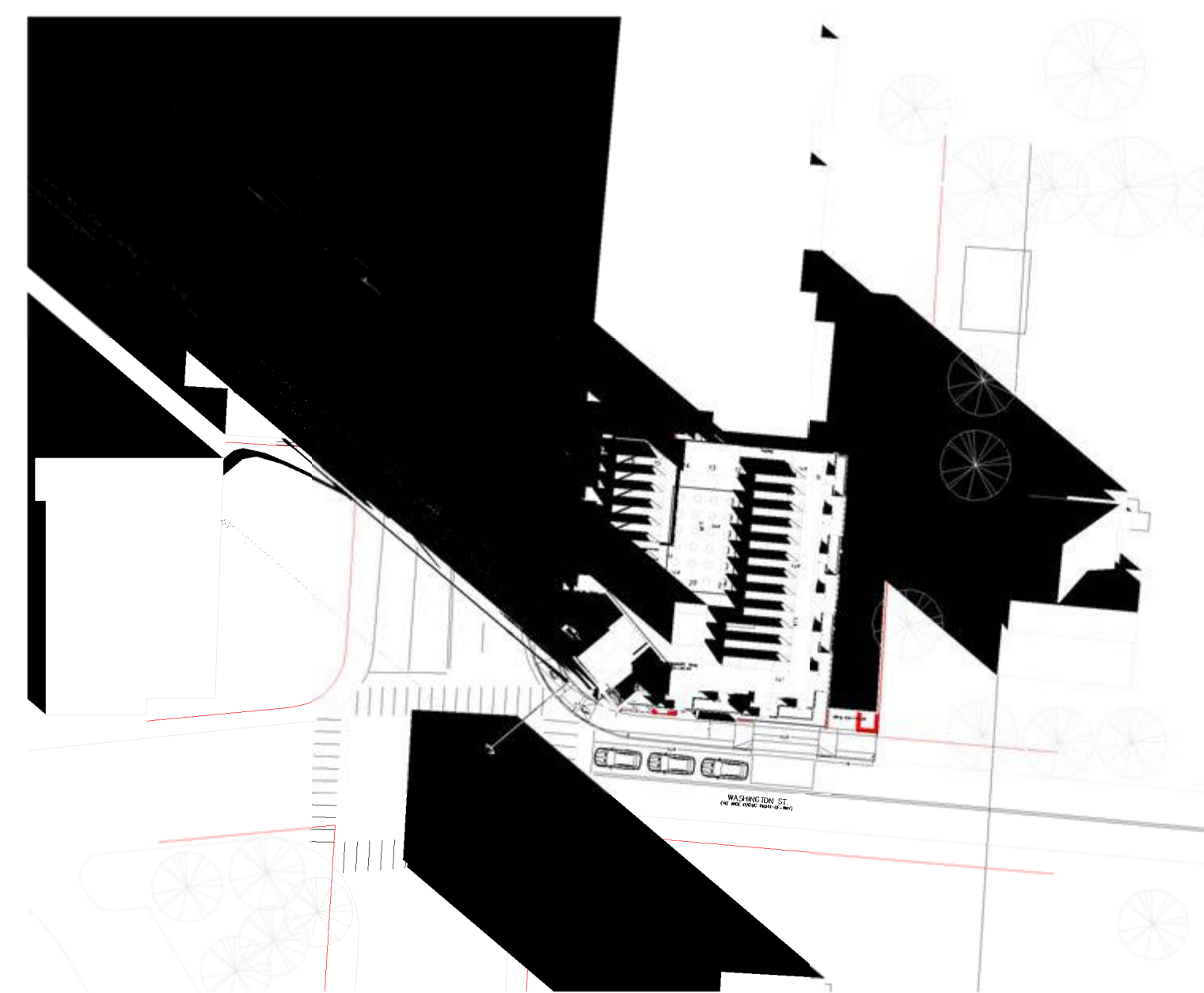
4 G002_SHADOW STUDY_09.20.0800
 1" = 50'-0"



5 G002_SHADOW STUDY_09.20.1200
 1" = 50'-0"



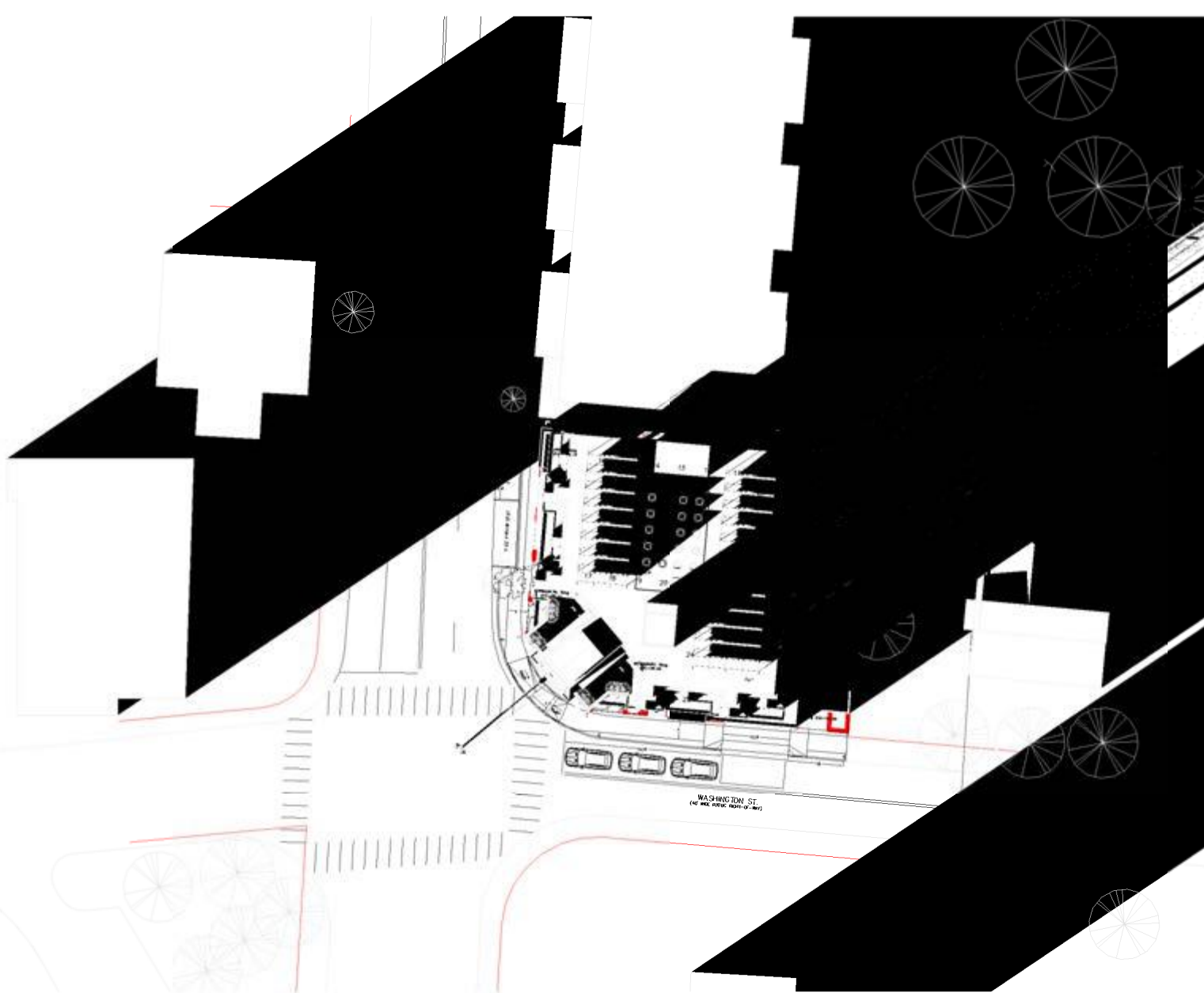
6 G002_SHADOW STUDY_09.20.1600
 1" = 50'-0"



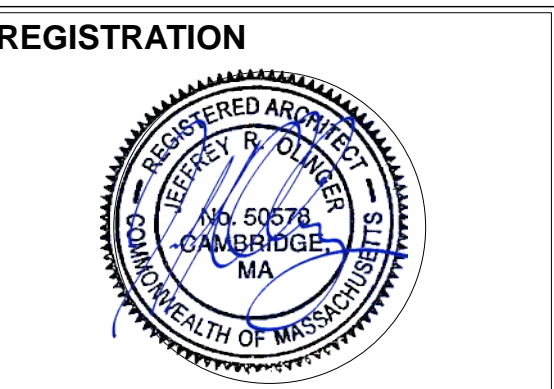
7 G002_SHADOW STUDY_12.20.0800
 1" = 50'-0"



8 G002_SHADOW STUDY_12.20.1200
 1" = 50'-0"



9 G002_SHADOW STUDY_12.20.1600
 1" = 50'-0"



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	1" = 50'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

SHADOW STUDY

G003

459 MAIN STREET



1 G004_4TH FLOOR TERRACE EXTERIOR



2 G004_VIEW FROM CORNER PARK



3 G004_MAIN VIEW



4 G004_WASHINGTON VIEW

PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

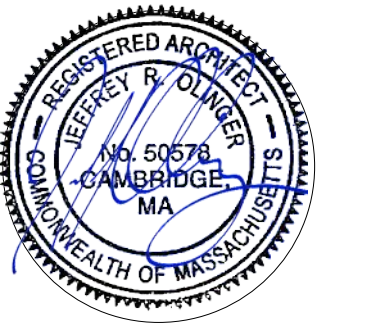
PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492

REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
3	ZONING UPDATE	04/05/2022

PROJECT VIEWS

G004

459 MAIN STREET



PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

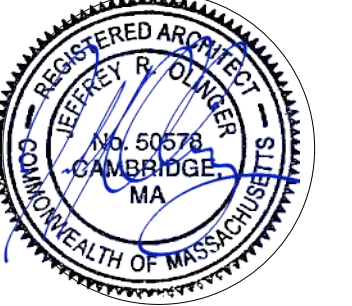
PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492

REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
4	ZONING UPDATE	05/31/2022

GROUND LEVEL AXO

G005

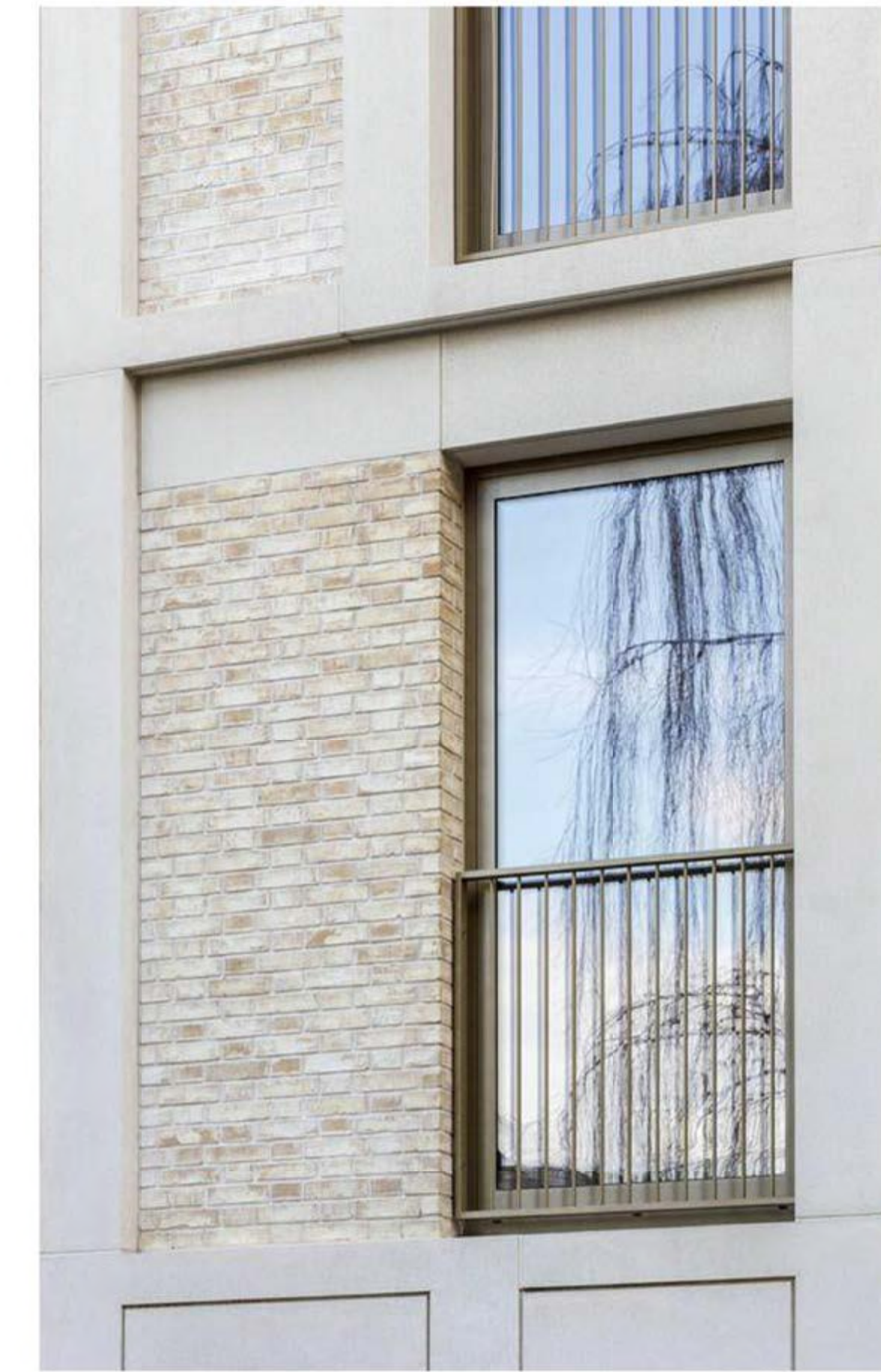
459 MAIN STREET



NAVY BLUE SHIPLAP SIDING



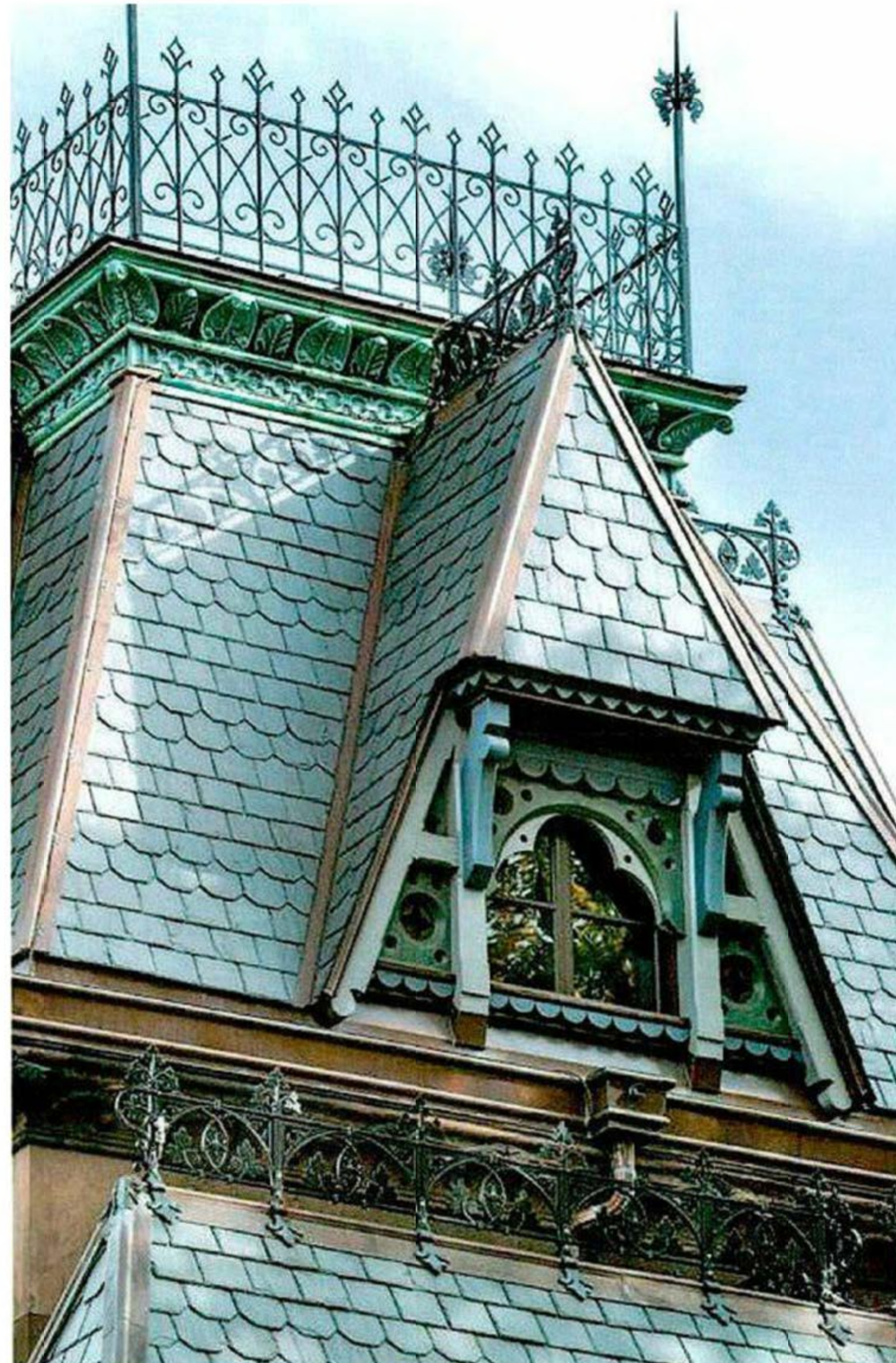
PATINA STANDING SEAM



LIGHT COLORED MASONRY



EXAMPLE: 'SECOND EMPIRE' WITH NAVY BLUE SHIPLAP SIDING



EXAMPLE: 'SECOND EMPIRE' WITH GREEN SLATE MANSARD



EXAMPLE: 'SECOND EMPIRE' WITH LIGHT COLORED MASONRY

PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

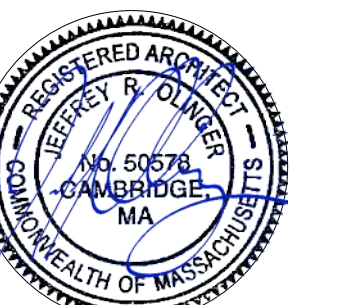
APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects

77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492

REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

PROJECT MATERIALS BOARD

G006
 459 MAIN STREET

PROJECT NAME
459 MAIN STREET REDEVELOPMENT

PROJECT ADDRESS:
 459 MAIN STREET, READING, MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



1 G007_CONTEXT ELEVATION _ WEST
 1" = 10'-0"



2 G007_CONTEXT ELEVATION _ EAST
 1" = 10'-0"



ZONING REVIEW

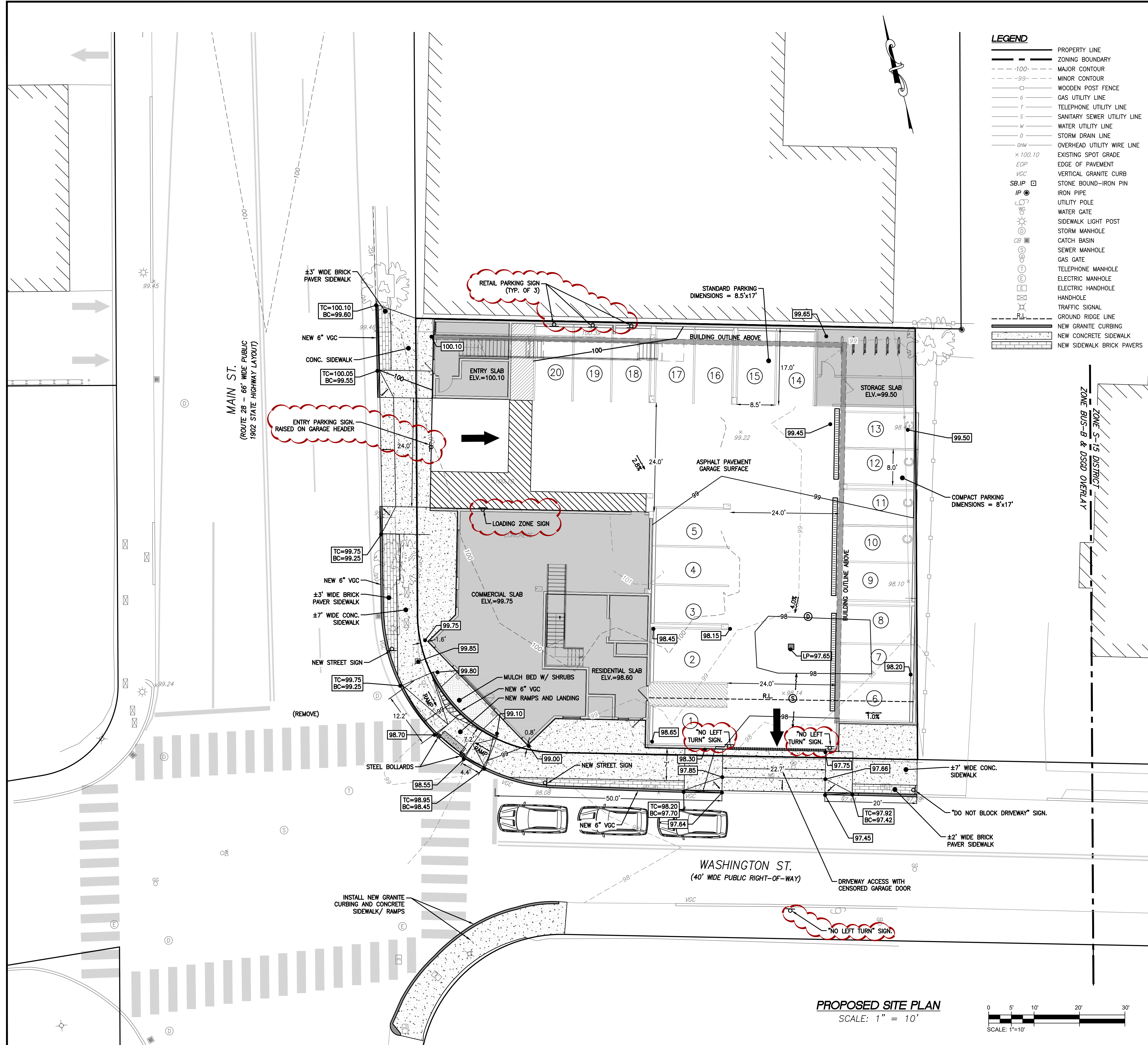
Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	1" = 10'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

CONTEXT ELEVATIONS

G007
 459 MAIN STREET



- LEGEND**
- PROPERTY LINE
 - - - ZONING BOUNDARY
 - - -100- - MAJOR CONTOUR
 - - -99- - MINOR CONTOUR
 - WOODEN POST FENCE
 - G GAS UTILITY LINE
 - T TELEPHONE UTILITY LINE
 - S SANITARY SEWER UTILITY LINE
 - W WATER UTILITY LINE
 - D STORM DRAIN LINE
 - DHW OVERHEAD UTILITY WIRE LINE
 - ×100.10 EXISTING SPOT GRADE
 - EDP EDGE OF PAVEMENT
 - VCC VERTICAL GRANITE CURB
 - SB,IP □ STONE BOUND-IRON PIN
 - IP ○ IRON PIPE
 - UTILITY POLE
 - WATER GATE
 - SIDEWALK LIGHT POST
 - STORM MANHOLE
 - CATCH BASIN
 - SEWER MANHOLE
 - GAS GATE
 - TELEPHONE MANHOLE
 - ELECTRIC MANHOLE
 - ELECTRIC HANDHOLE
 - HANDHOLE
 - TRAFFIC SIGNAL
 - GROUND RIDGE LINE
 - NEW GRANITE CURBING
 - NEW CONCRETE SIDEWALK
 - NEW SIDEWALK BRICK PAVERS

- SITE AND GRADING NOTES**
- ALL WALKWAYS SHALL CONSIST OF CONCRETE.
 - ALL CONSTRUCTION MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE TO THE MUNICIPALITY'S LATEST CONSTRUCTION SPECIFICATIONS AND DETAILS.
 - WORK WITHIN ROUTE 28 SHALL REQUIRE SEPARATE PERMITTING FROM MASSDOT.
 - THE CONTRACTOR SHALL CAREFULLY PRESERVE BENCHMARKS, REFERENCE POINTS AND STAKES.
 - ALL INDICATED ELEVATIONS ARE FINISHED ELEVATIONS.
 - IF AREAS ARE DISTURBED BEYOND PROPOSED GRADES BY NEGLIGENCE OF THE CONTRACTOR, THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY REGRADING OR REPAIR TO MATCH ORIGINAL EXISTING CONDITIONS.
 - CONTRACTOR SHALL ADJUST GRADES BY VARYING THE PAVEMENT SECTIONS ACCORDINGLY. EXISTING COMPACTED SUBGRADE TO BE DISTURBED AS LITTLE AS POSSIBLE.
 - ALL PROPOSED SPOT ELEVATIONS SHOWN INDICATE FINISHED GRADED ELEVATIONS AT EDGE OF PAVEMENT AND/OR GRADE BREAKS, UNLESS OTHERWISE NOTED.
 - MAINTAIN PROPER SITE DRAINAGE AT ALL TIMES DURING THE COURSE OF CONSTRUCTION, AND PREVENT STORM WATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS.
 - SPREAD AND COMPACT UNIFORMITY TO THE DEGREE SPECIFIED ALL EXCESS TRENCH SPOIL AFTER COMPLETION OF THE UNDERGROUND IMPROVEMENTS (EARTHWORK CONTRACTOR SHALL MAKE APPROPRIATE ADJUSTMENTS IN ROUGH GRADING TO ACCOMMODATE TRENCH SPOIL).
 - PROVIDE WATER TO ADD TO DRY MATERIAL IN ORDER TO ADJUST THE MOISTURE CONTENT FOR THE PURPOSE OF ACHIEVING THE SPECIFIED COMPACTION.
 - REMOVE ALL UNSUITABLE MATERIAL AND REPLACE WITH SUITABLE STRUCTURAL FILL.
 - REPAIR ANY BASE COURSE AND BINDER COURSE FAILURES PRIOR TO THE INSTALLATION OF THE FINAL BITUMINOUS CONCRETE SURFACE COURSE.
 - SWEEP CLEAN THE BINDER COURSE PRIOR TO THE INSTALLATION OF THE FINAL BITUMINOUS CONCRETE SURFACE COURSE. EXCESSIVE CLEANING OF THE BINDER COURSE THAT MAY BE REQUIRED, AND IS NOT THE FAULT OF THE PAVING CONTRACTOR, SHALL BE PAID FOR ON A TIME AND MATERIAL BASIS BY PRIOR AGREEMENT WITH THE GENERAL CONTRACTOR.
 - A GEOTEXTILE MATTING (LANDLOCK TRM 450 OR EQUIVALENT) SHALL BE USED FOR EROSION CONTROL AND ALL SLOPES GREATER THAN 3H:1V IF NECESSARY.
 - IF THE CONTRACTOR IN THE COURSE OF WORK FINDS ANY DISCREPANCIES BETWEEN THE PLANS AND THE PHYSICAL CONDITIONS OF THE LOCALITY, OR ANY ERRORS OR OMISSIONS IN THE PLANS OR IN THE LAYOUT AS GIVEN BY THE ENGINEER, IT SHALL BE HIS DUTY TO IMMEDIATELY INFORM THE ENGINEER, IN WRITING AND THE ENGINEER WILL PROMPTLY VERIFY THE SAME. ANY WORK DONE AFTER SUCH A DISCOVERY, UNTIL AUTHORIZED, WILL BE AT THE CONTRACTOR'S RISK.

PARKING SUMMARY
REQUIRED 1.25 SPOT PER DWELLING UNIT = 12 UNITS x 1.25 = 15 SPACES

	REQUIRED	PROPOSED	RELIEF REQUIRED
PARKING	15	*20	N

* INCLUDES TWO (2) ADA PARKING SPOTS.

ZONING SUMMARY
DOWNTOWN SMART GROWTH DISTRICT (DSGD)

	REQUIRED	PROVIDED	RELIEF REQUIRED
MAX FLOOR AREA RATIO (SF)	2.8 (28,716±)	(1)	N
MIN. LOT FRONTAGE	50'	192.57'	N
MAX LOT COVERAGE	N/A	91%	N
MIN LOT AREA	N/A	10,256 S.F.	N
NUMBER OF BUILDINGS PER LOT	N/A	1	N
MAXIMUM BUILDING FRONTAGE	300'	±190'	N
FRONT SETBACK (MIN./MAX)	0'/10'	0.8'	N
MIN. SIDE SETBACK	0'	0'	N
MIN. REAR SETBACK	0'	N/A	N
INTERIOR SETBACK BETWEEN BUILDINGS ON SAME LOT	15'	N/A	N
DWELLING UNIT DENSITY PER ACRE	20	51	Y

(1) REFER TO ARCHITECTURAL PLANS.

PREPARED BY:
FODERA ENGINEERING
Office: (617) 992-8492
contact@foderaengineering.com
28 Harbor St., Suite 204
Danvers, MA 01923

PREPARED FOR:
Giuseppe Fodera,
Gaetano Manganiello,
& Joseph Fodera
28 Harbor Street
Danvers, MA 01923

COPYRIGHT NOTICE
This drawing is property of Fodera Engineering and is not to be used for any purpose other than the specific project and site named herein, and cannot be reproduced in any manner without the express written permission from the engineer on record.

REVISION	DATE	BY
REV-12 UNITS	2/1/22	GGF
DRIVE PATH REV.	4/6/22	GGF
ENG. COMMENTS	6/1/22	GGF
CPDC REVISION	9/7/22	GGF

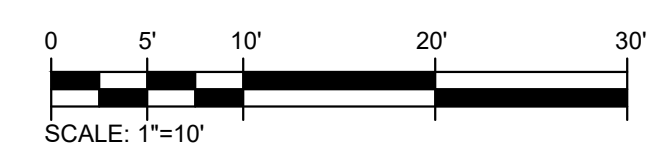
PROJECT LOCATION:
459 MAIN STREET
READING, MA 01867
PARCEL ID:
MAP 17, PARCEL 62

PLAN SET:
PROPOSED 40R DEVELOPMENT
459 MAIN STREET
READING, MA 01867

PROFESSIONAL SEAL:
COMMONWEALTH OF MASSACHUSETTS
GIOVANNI GAETANO FODERA
No. 54884
REGISTERED PROFESSIONAL ENGINEER

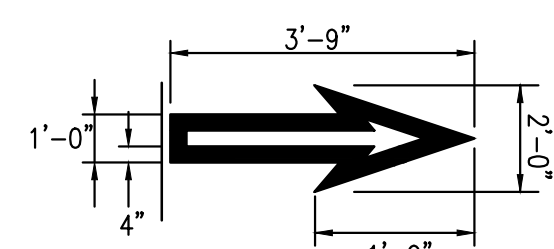
JOB NO.: 21228-238
SHEET TITLE:
PROPOSED SITE PLAN
SHEET NUMBER:
C-2

PROPOSED SITE PLAN
SCALE: 1" = 10'



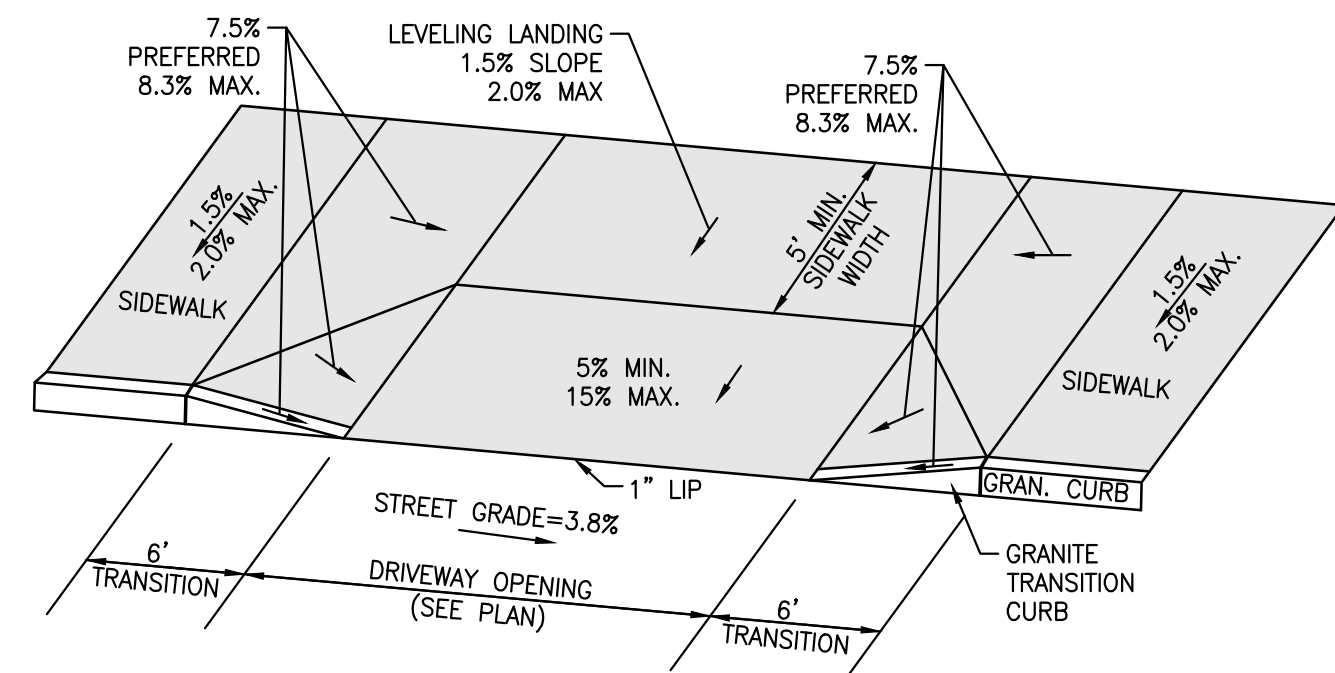
DECEMBER 1, 2021

SCALE: 1" = 10'

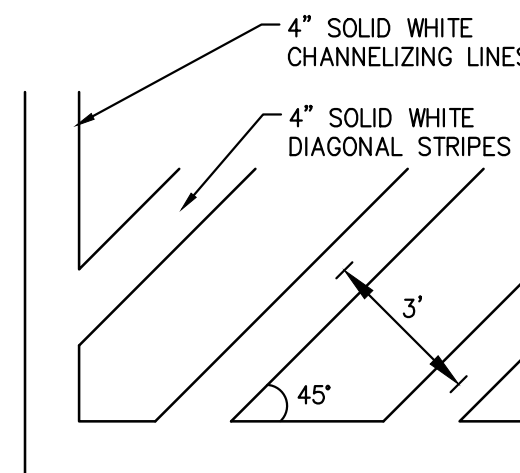


NOTES:
ALL TRAFFIC FLOW ARROWS TO BE REFLECTIVE 5 MIL. WHITE PAINT PER SPECS

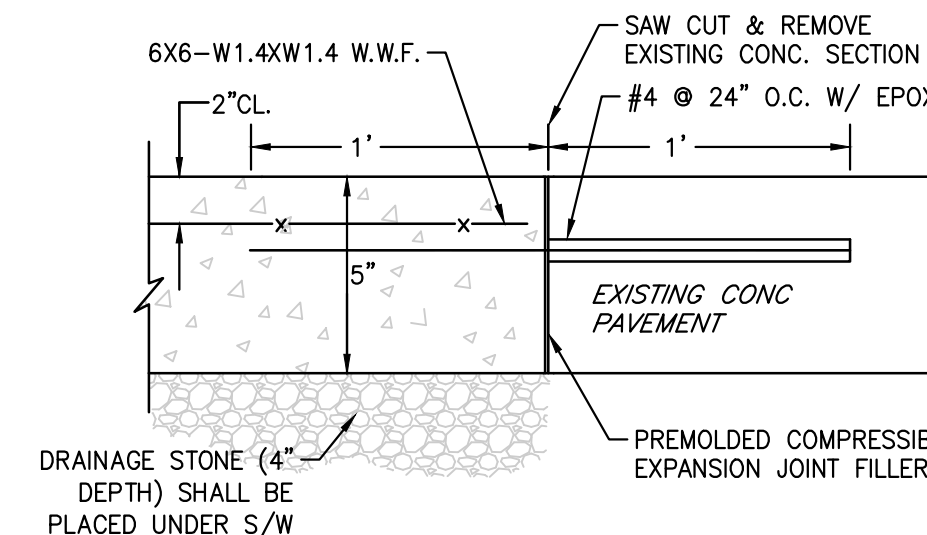
DIRECTIONAL ARROW DETAIL
N.T.S.



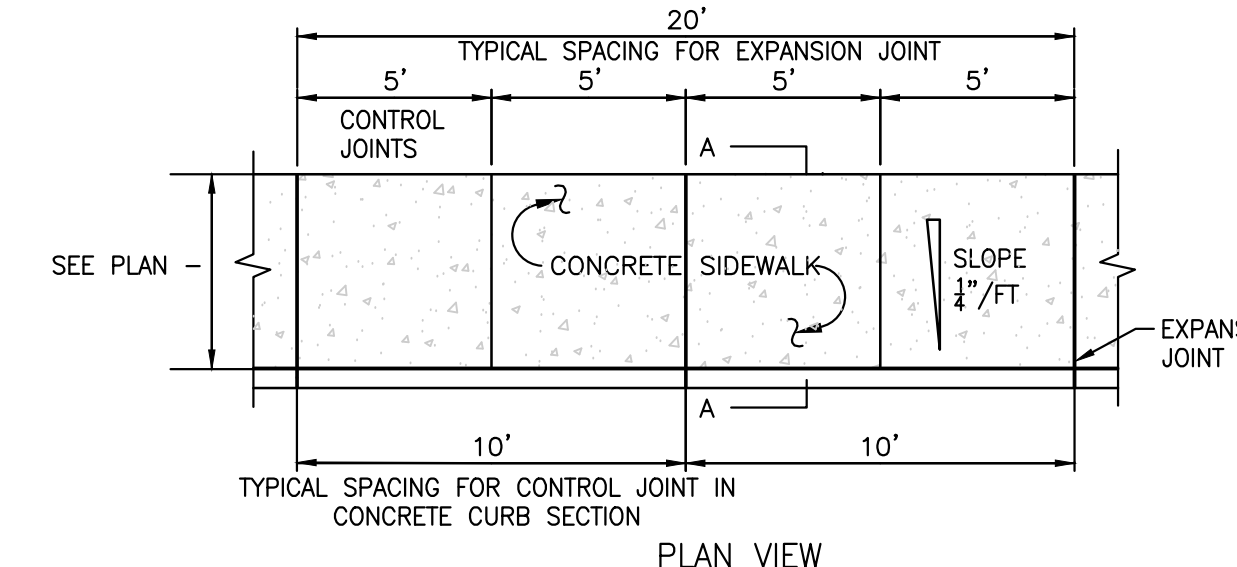
CONCRETE DRIVEWAY APRON
N.T.S.



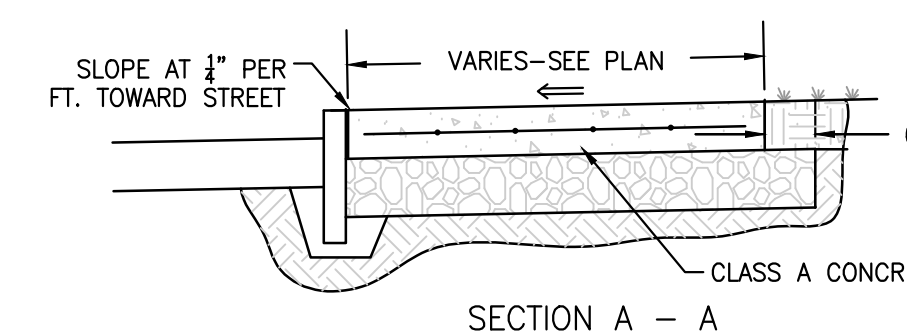
TYPICAL PAVEMENT PAINTING SECTION
N.T.S.



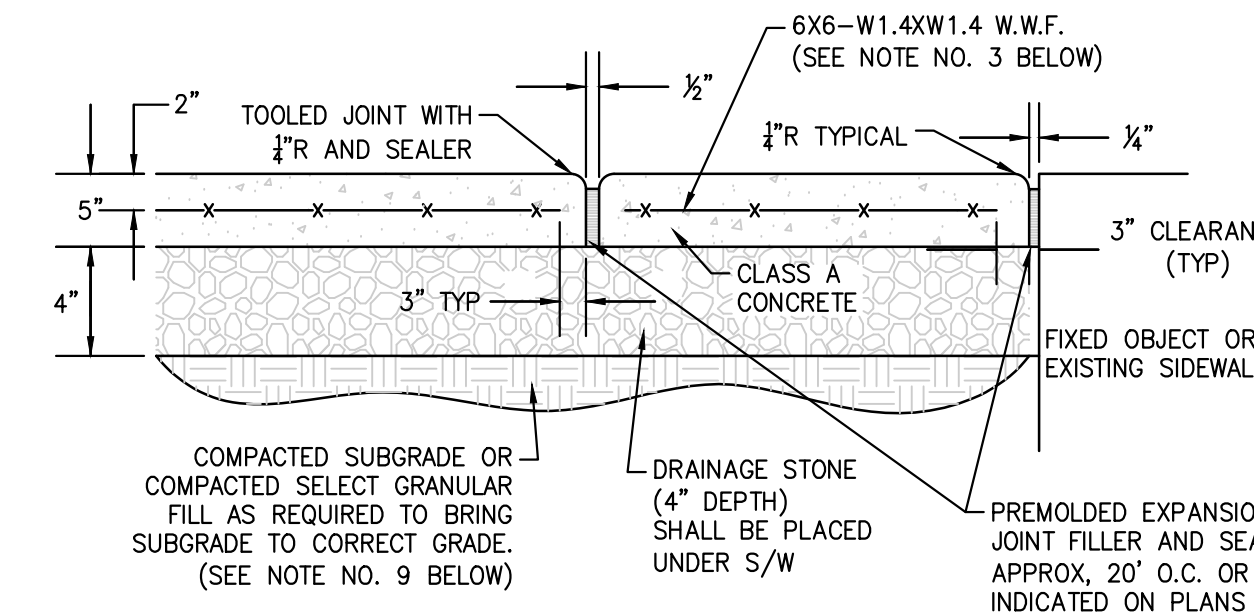
CONCRETE REMOVAL AND REPLACEMENT
N.T.S.



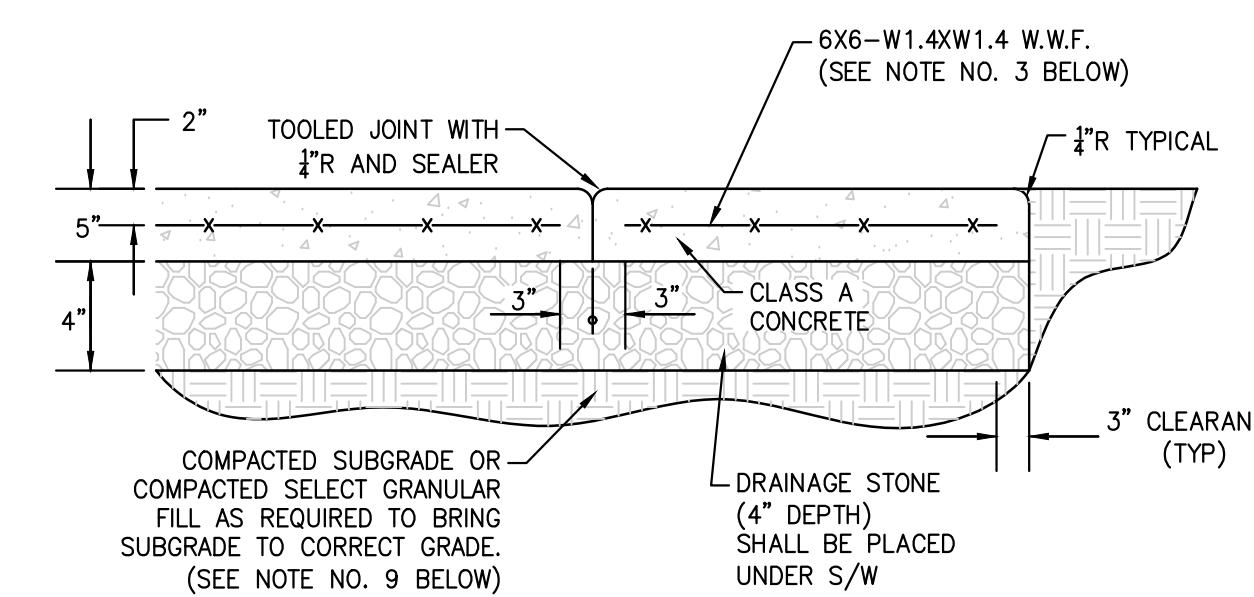
NOTES:
1. 10' SPACING OF CONTROL JOINTS SHALL BE USED FOR CAST IN PLACE CONCRETE CURBS.



SECTION A - A



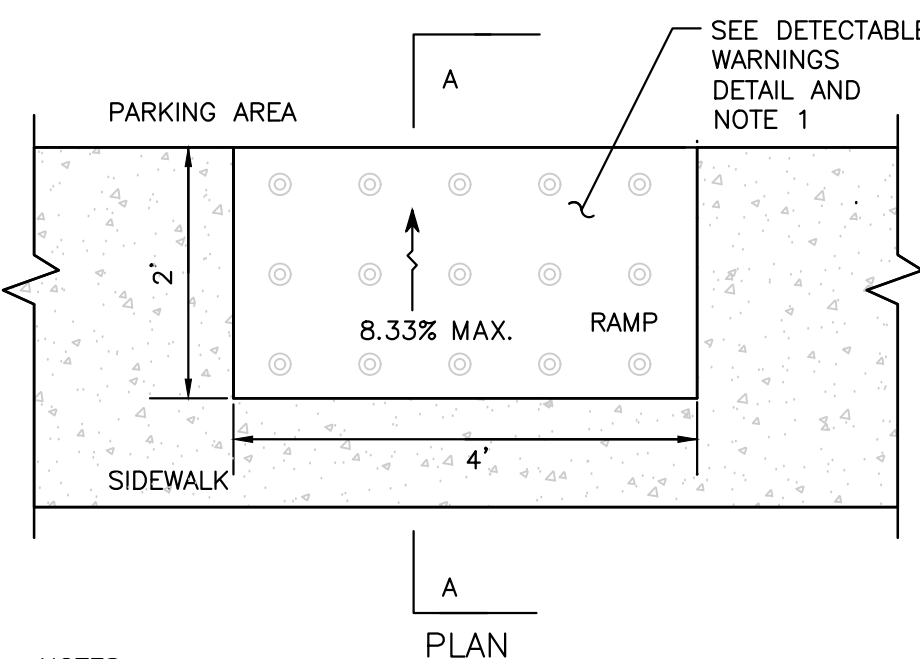
EXPANSION JOINT



CONSTRUCTION JOINT

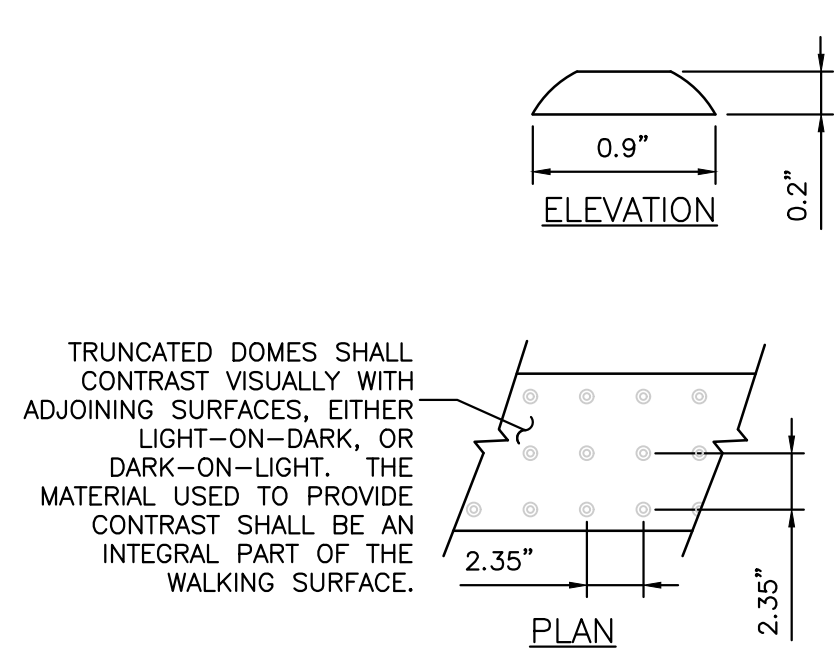
- NOTES:
- MATERIAL AND METHODS OF CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE STATE DEPARTMENT OF TRANSPORTATION.
 - ALL CONCRETE SHALL BE 3,000 PSI CLASS A CONCRETE. CONCRETE MATERIALS, PLACEMENT, AND CONSTRUCTION SHALL CONFORM STATE DOT STANDARD SPECIFICATIONS.
 - REINFORCING STEEL FOR CONCRETE SHALL CONFORM WITH THE ABOVE REFERENCED STATE DOT STANDARD SPECIFICATIONS AND AS SPECIFICALLY CALLED OUT IN THE DRAWINGS.
 - ALL EXPOSED SURFACES SHALL HAVE A BROOM TEXTURED FINISH.
 - EXPANSION JOINTS SHALL BE LOCATED A MAXIMUM OF 20'-0" ON CENTER, OR AS INDICATED ON PLANS.
 - JOINTS SHALL NOT BE SAW CUT.
 - EXPOSED CONCRETE SURFACES SHALL BE TREATED WITH "SUREBOND"/"SAFEbond" SB-7000 INTENSIFIER BRIGHT PROTECTOR AND SIDEWALK SEALER, OR EQUAL. RATE AND METHOD OF APPLICATION SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
 - SUBBASE MATERIAL SHALL CONFORM WITH STATE DOT STANDARD.
 - WHERE IT IS NECESSARY TO PLACE FILL FOR PURPOSE OF BRINGING THE SUBGRADE ELEVATION UP TO A SPECIFIED GRADE, THE FILL MATERIAL PLACED SHALL BE IN CONFORMANCE WITH STATE DOT STANDARD SPECIFICATIONS.
 - CURB SHALL BE REVEALED BY 6 INCHES ABOVE GRADE UNLESS OTHERWISE NOTED ON THE PLAN.

SIDEWALK DETAIL
N.T.S.



NOTES:
1. THE SURFACE OF RAMP SHALL HAVE DETECTABLE WARNINGS. DETECTABLE WARNINGS SHALL CONSIST OF RAISED TRUNCATED DOMES. SURFACE SHALL DIFFER FROM ADJOINING WALKING SURFACES IN RESILIENCY OR SOUND-ON-CANE CONTACT. SEE DETECTABLE WARNINGS DETAIL.
2. CONSTRUCT PER A.D.A. STANDARDS.

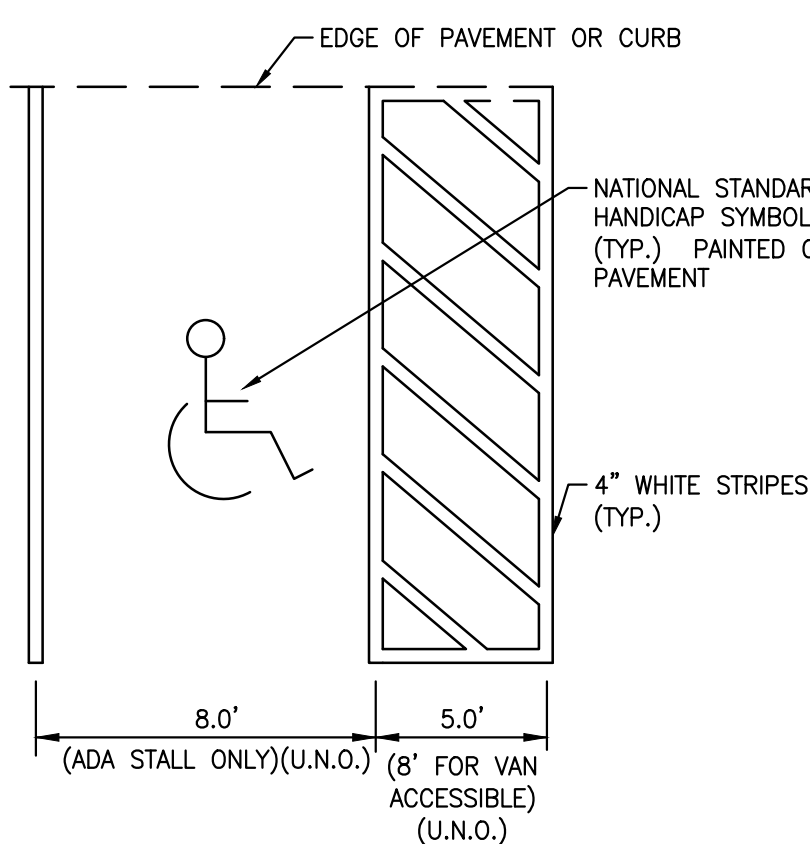
ADA WARNING STRIP DETAIL
N.T.S.



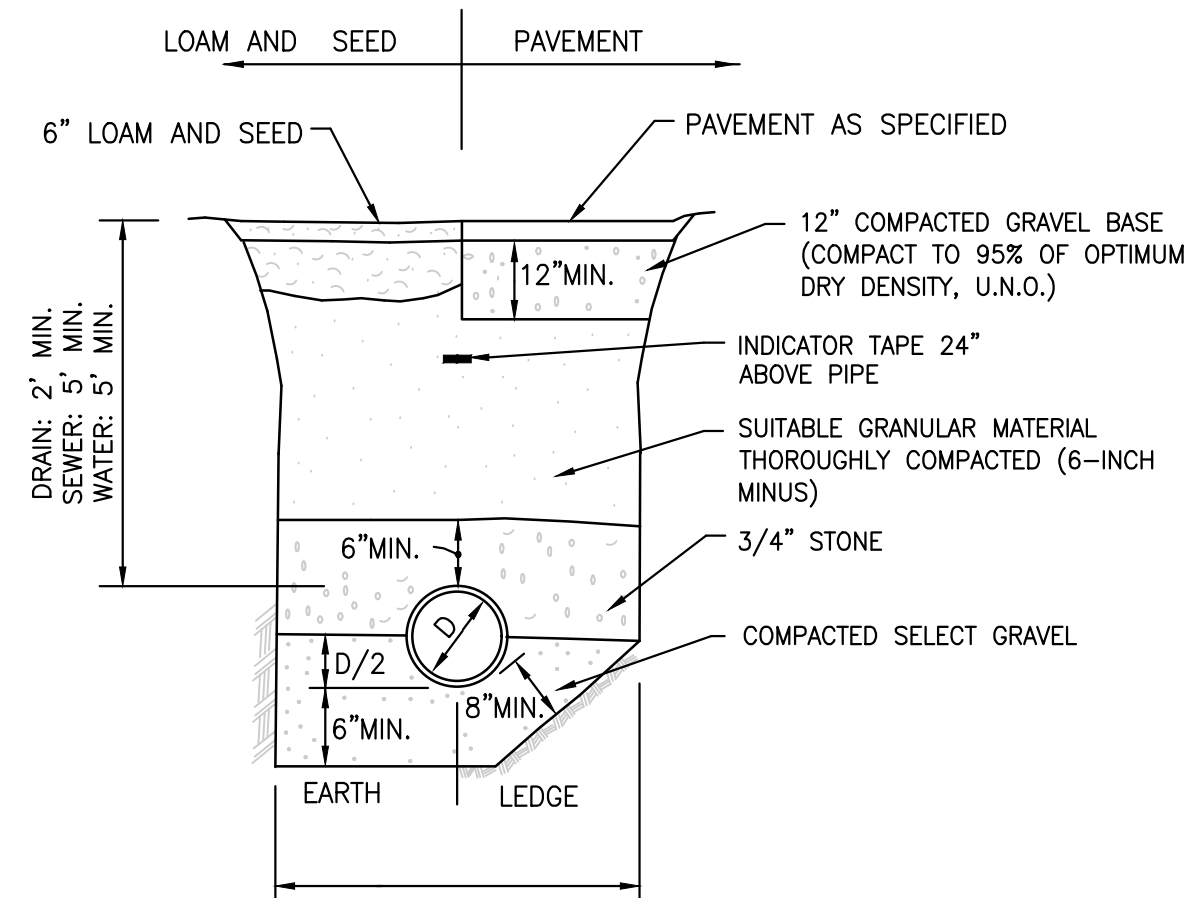
TRUNCATED DOMES SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT. THE MATERIAL USED TO PROVIDE CONTRAST SHALL BE AN INTEGRAL PART OF THE WALKING SURFACE.

* PER A.D.A. ACCESSIBILITY GUIDELINES SECTION 4.29.2 & A4.29.2

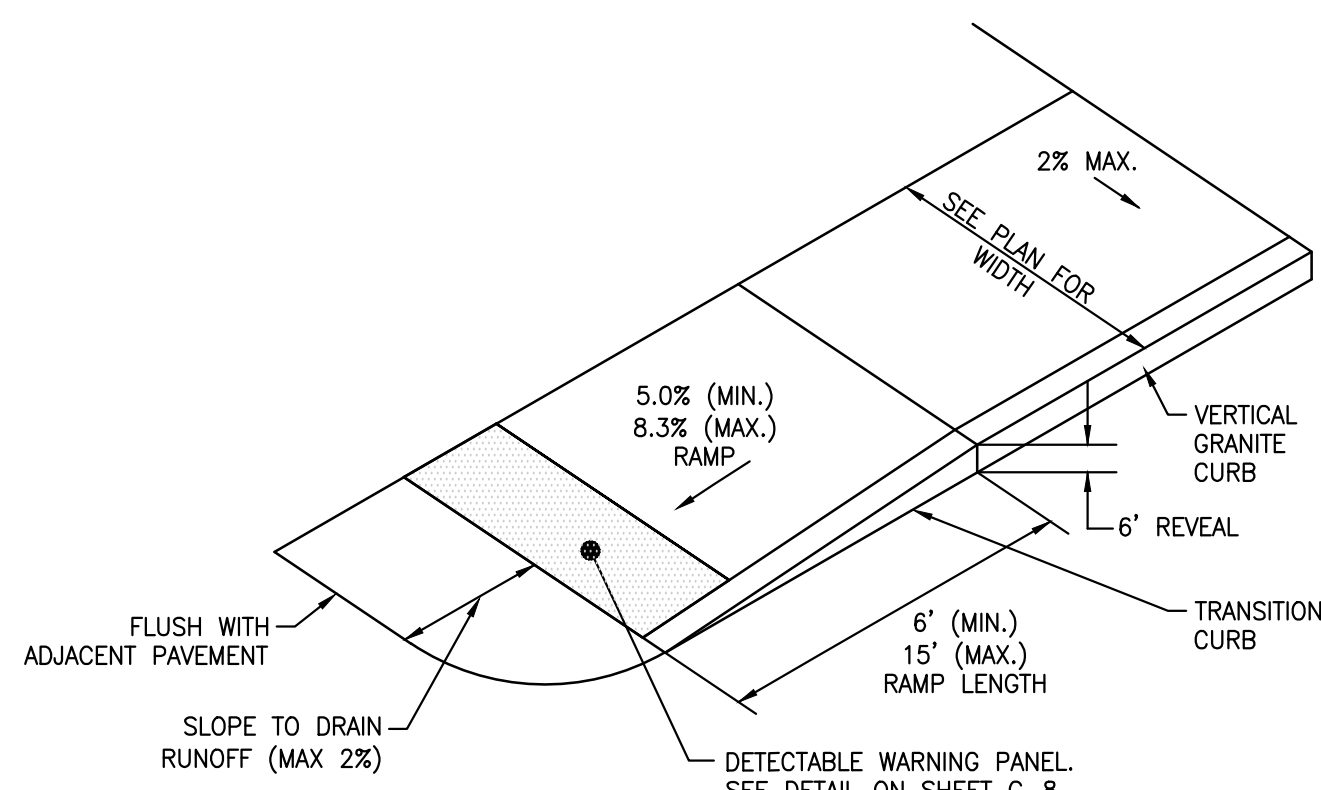
DETECTABLE WARNINGS DETAIL



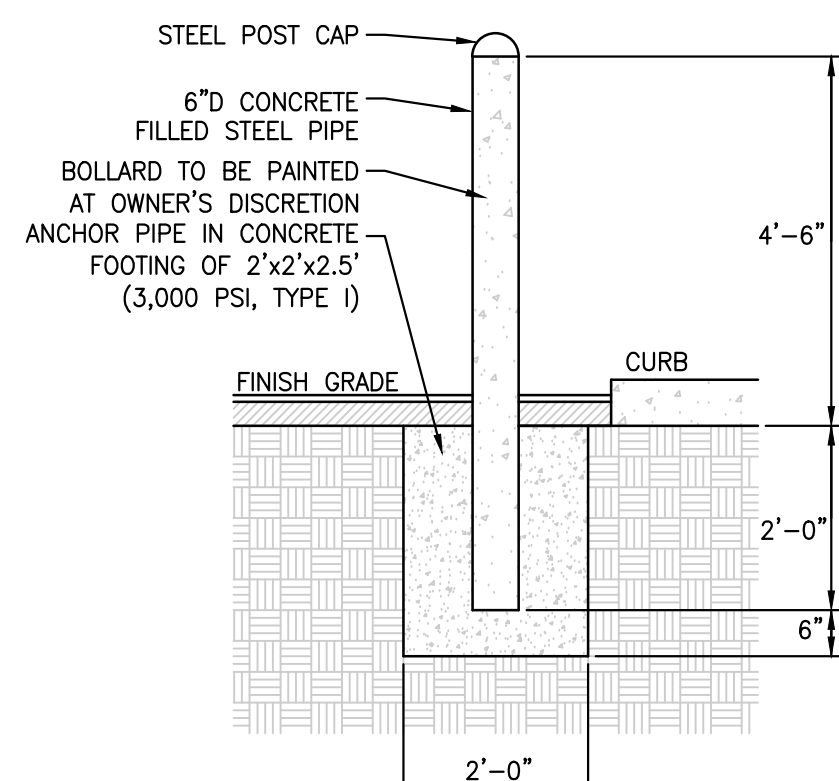
ADA PARKING STALL
N.T.S.



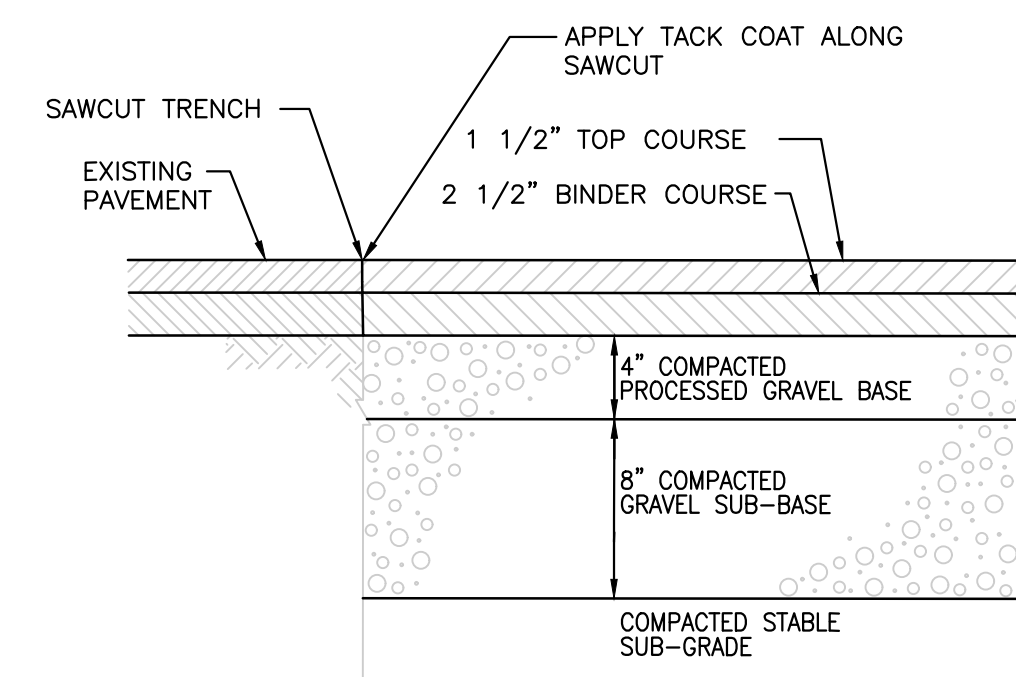
DRAIN/ SEWER/ WATER TRENCH AND BACKFILL DETAIL
N.T.S.



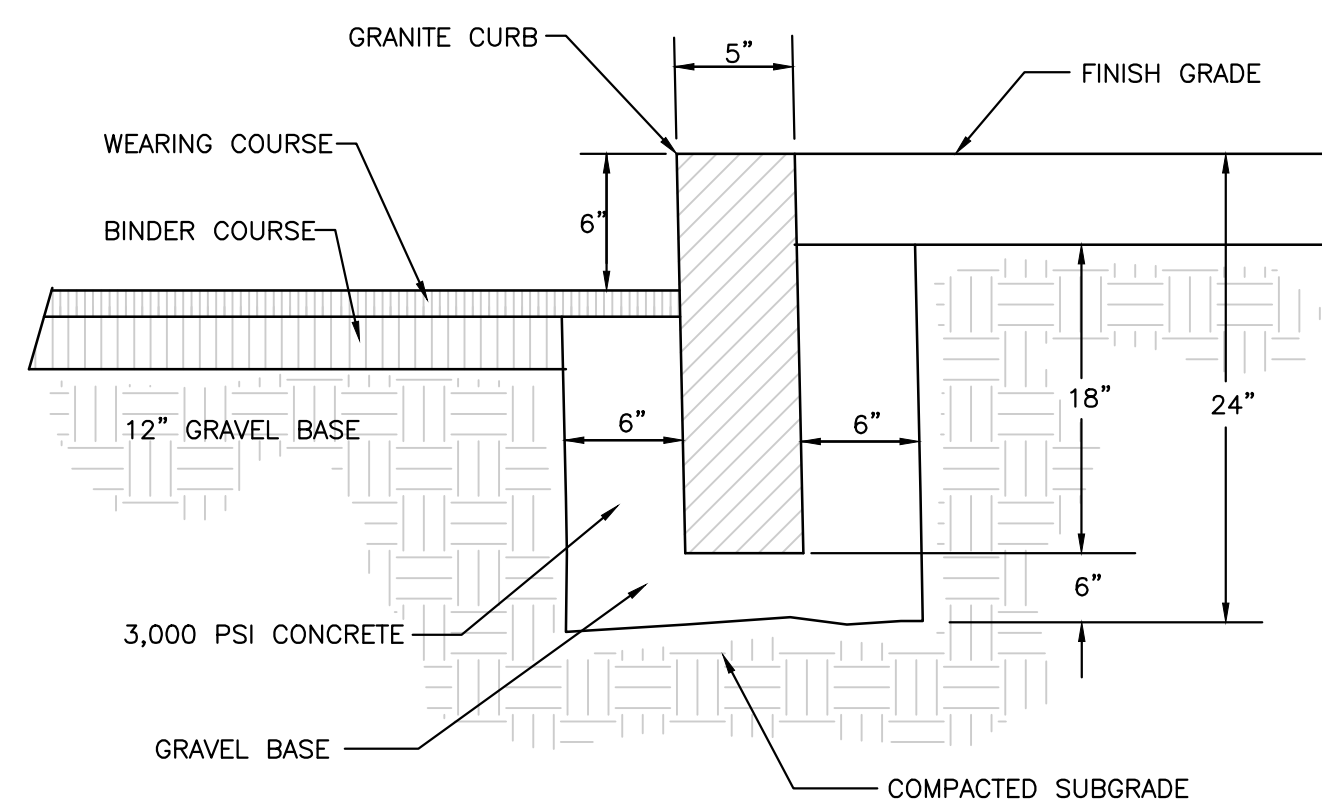
ADA ACCESSIBLE SIDEWALK RAMP
N.T.S.



STEEL PIPE BOLLARD
N.T.S.



TYPICAL BITUMINOUS PAVEMENT SECTION
N.T.S.



VERTICAL GRANITE CURB
N.T.S.

PREPARED BY:
FODERA ENGINEERING
Office: (617) 992-8492
contact@foderaengineering.com
28 Harbor St., Suite 204
Danvers, MA 01923

PREPARED FOR:
Giuseppe Fodera,
Gaetano Manganiello,
& Joseph Fodera

28 Harbor Street
Danvers, MA 01923

COPYRIGHT NOTICE
This drawing is property of Fodera Engineering and is not to be used for any purpose other than the specific project and site named herein, and cannot be reproduced in any manner without the express written permission from the engineer on record.

REVISION	DATE	BY
REV-12 UNITS	2/1/22	GGF
DRIVE PATH REV.	4/6/22	GGF
ENG. COMMENTS	6/1/22	GGF
CPDC REVISION	9/7/22	GGF

PROJECT LOCATION:
459 MAIN STREET
READING, MA 01867
PARCEL ID:
MAP 17, PARCEL 62

PLAN SET:
PROPOSED 40R DEVELOPMENT
459 MAIN STREET
READING, MA 01867

PROFESSIONAL SEAL:
COMMONWEALTH OF MASSACHUSETTS
GIOVANNI GAETANO FODERA
No. 54884
REGISTERED PROFESSIONAL ENGINEER

JOB NO.: 21228-238
SHEET TITLE:
DETAILS SHEET

SHEET NUMBER:
C-4

DECEMBER 1, 2021

SCALE: N.T.S.

COPYRIGHT NOTICE
 This drawing is property of Fodera Engineering and is not to be used for any purpose other than the specific project and site named herein, and cannot be reproduced in any manner without the express written permission from the engineer on record.

REVISION	DATE	BY
REV-12 UNITS	2/1/22	GGF
DRIVE PATH REV.	4/6/22	GGF
ENG. COMMENTS	6/1/22	GGF
CPDC REVISION	9/7/22	GGF

PROJECT LOCATION:
 459 MAIN STREET
 READING, MA 01867
PARCEL ID:
 MAP 17, PARCEL 62

PLAN SET:
 PROPOSED 40R DEVELOPMENT
 459 MAIN STREET
 READING, MA 01867
 DECEMBER 1, 2021

SCALE: N.T.S.

PROFESSIONAL SEAL:
 COMMONWEALTH OF MASSACHUSETTS
 GIOVANNI GAETANO FODERA
 No. 54884
 REGISTERED PROFESSIONAL ENGINEER

JOB NO.: 21228-238
SHEET TITLE:
 DETAILS SHEET

SHEET NUMBER:
 C-4

SPECIFICATION CLAUSE
S300K POWERDRAIN - LOAD CLASS A

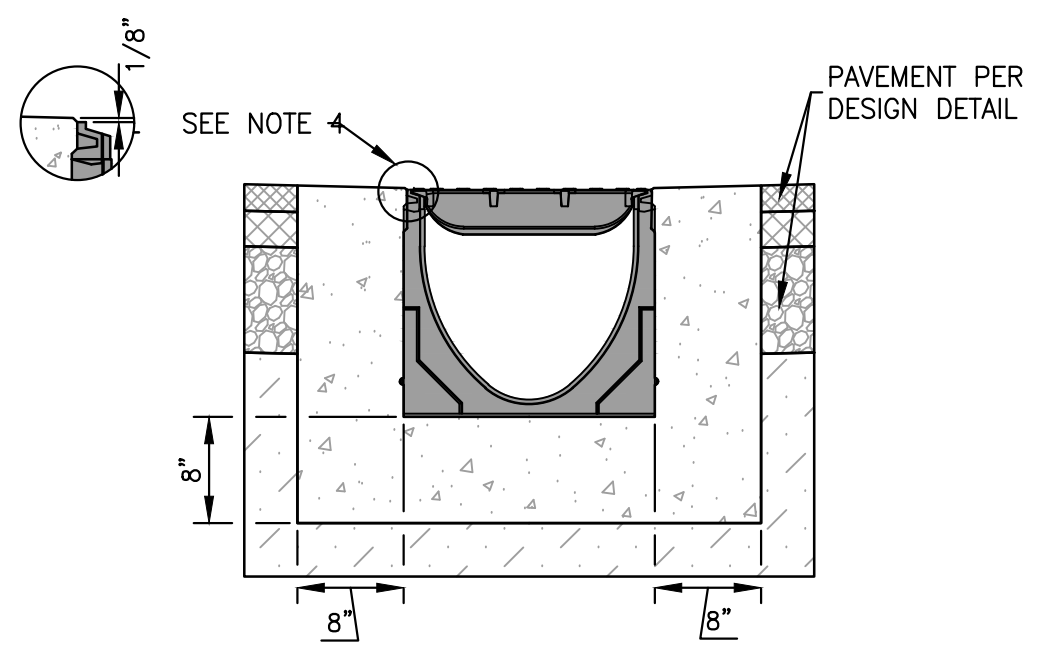
GENERAL
 THE SURFACE DRAINAGE SYSTEM SHALL BE POLYMER CONCRETE S300K CHANNEL SYSTEM WITH DUCTILE IRON EDGE RAILS AS MANUFACTURED BY ACO POLYMER PRODUCTS, INC.

MATERIALS
 CHANNELS SHALL BE MANUFACTURED FROM POLYESTER RESIN POLYMER CONCRETE WITH AN INTEGRALLY CAST-IN DUCTILE IRON EDGE RAIL. MINIMUM PROPERTIES OF POLYMER CONCRETE WILL BE AS FOLLOWS:
 COMPRESSIVE STRENGTH: 14,000 PSI
 FLEXURAL STRENGTH: 4,000 PSI
 TENSILE STRENGTH: 1,500 PSI
 WATER ABSORPTION: 0.07%
 FROST PROOF: YES
 DILUTE ACID AND ALKALI RESISTANT: YES
 B117 SALT SPRAY TEST COMPLIANT: YES

THE SYSTEM SHALL BE 12" (300mm) NOMINAL INTERNAL WIDTH WITH A 14.2" (390mm) OVERALL WIDTH AND A BUILT-IN SLOPE OF 0.5%. CHANNEL INVERT SHALL HAVE DEVELOPED "V" SHAPE. ALL CHANNELS SHALL BE INTERLOCKING WITH A MALE/FEMALE JOINT.

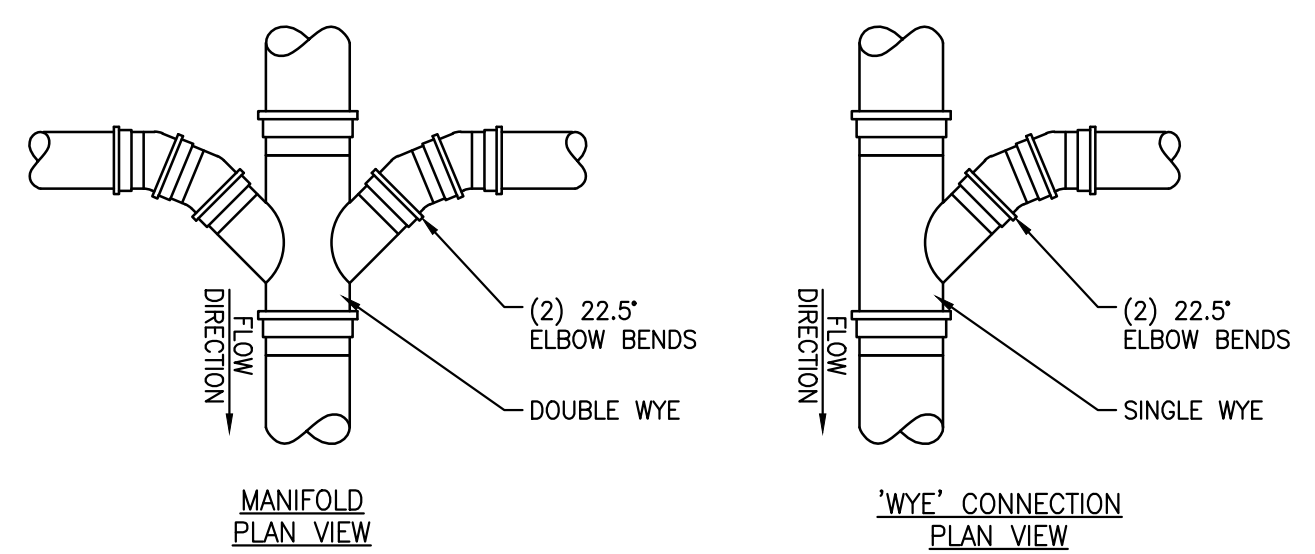
THE COMPLETE DRAINAGE SYSTEM SHALL BE BY ACO POLYMER PRODUCTS, INC. ANY DEVIATION OR PARTIAL SYSTEM DESIGN AND/OR IMPROPER INSTALLATION WILL VOID ANY AND ALL WARRANTIES PROVIDED BY ACO POLYMER PRODUCTS, INC.

CHANNEL SHALL WITHSTAND LOADING TO PROPER LOAD CLASS AS OUTLINED BY EN 1433. GRATE TYPE SHALL BE APPROPRIATE TO MEET THE SYSTEM LOAD CLASS SPECIFIED AND INTENDED APPLICATION. GRATES SHALL BE SECURED USING "POWERLOK" BOLTLESS LOCKING SYSTEM. CHANNEL AND GRATE SHALL BE CERTIFIED TO MEET THE SPECIFIED EN 1433 LOAD CLASS. THE SYSTEM SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS AND RECOMMENDATIONS.

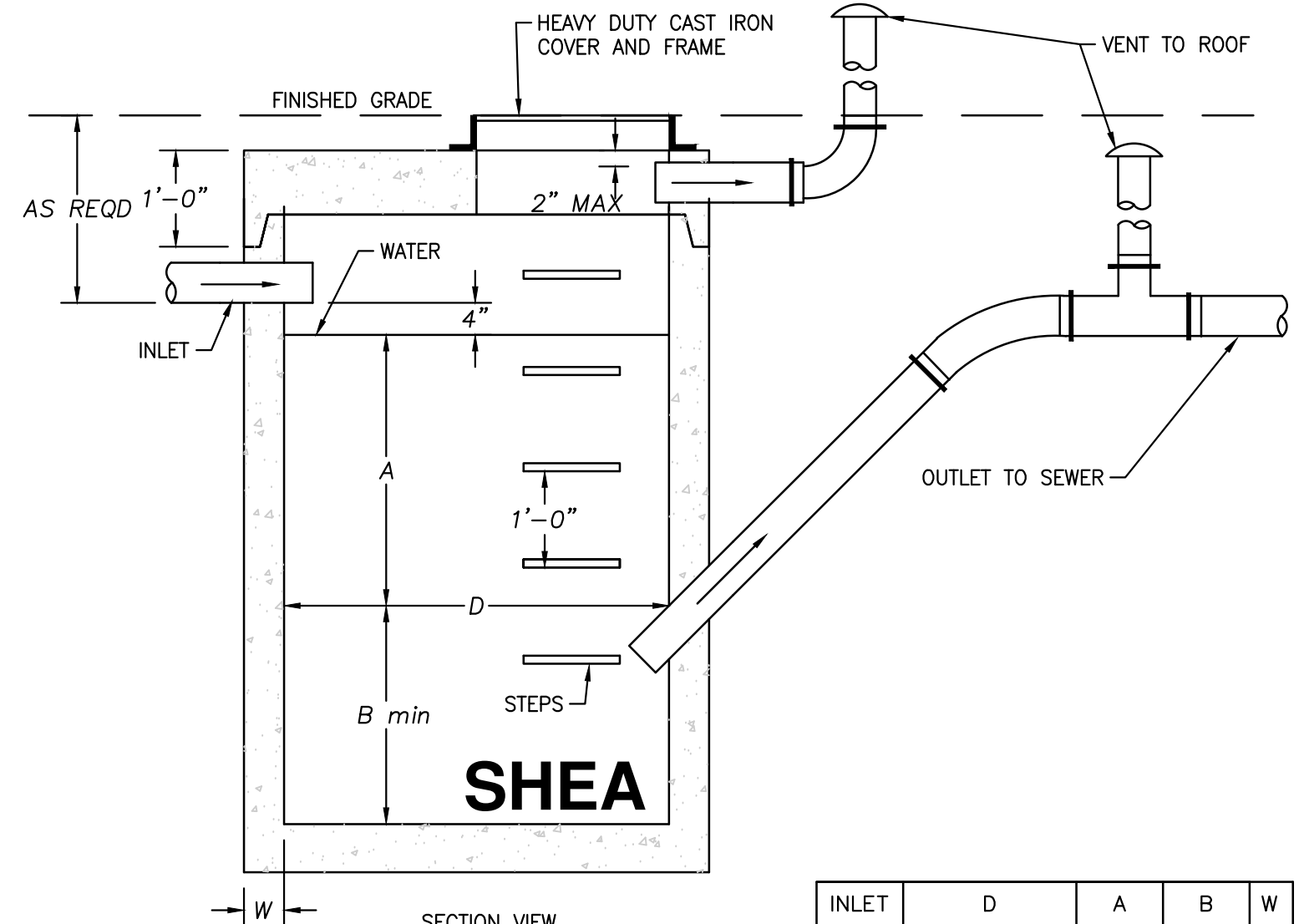


- NOTES:**
- IT IS NECESSARY TO ENSURE MINIMUM DIMENSIONS SHOWN ARE SUITABLE FOR EXISTING GROUND CONDITIONS. ENGINEERING ADVICE MAY BE REQUIRED.
 - MINIMUM CONCRETE STRENGTH OF 4,000 PSI IS RECOMMENDED. CONCRETE SHOULD BE VIBRATED TO ELIMINATE AIR POCKETS.
 - EXPANSION AND CONTRACTION CONTROL JOINTS AND REINFORCEMENT ARE RECOMMENDED TO PROTECT CHANNEL AND CONCRETE SURROUND. ENGINEERING ADVICE MAY BE REQUIRED.
 - THE FINISHED LEVEL OF THE CONCRETE SURROUND MUST BE APPROX. 1/8" [3mm] ABOVE THE TOP OF THE CHANNEL EDGE.
 - CONCRETE BASE THICKNESS SHOULD MATCH SLAB THICKNESS. ENGINEERING ADVICE MAY BE REQUIRED TO DETERMINE PROPER LOAD CLASS.
 - REFER TO ACO'S LATEST INSTALLATION INSTRUCTIONS FOR FURTHER DETAILS.

S300K POWERDRAIN TRENCH DRAIN
 N.T.S.



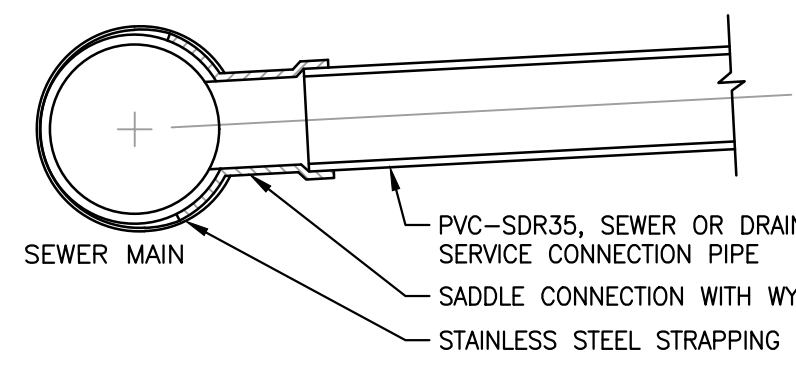
MANIFOLD AND WYE CONNECTIONS
 N.T.S.



- NOTES:**
- CONCRETE: 4,000 PSI MINIMUM AFTER 28 DAYS.
 - MANHOLE DESIGN SPECIFICATIONS CONFORM TO LATEST ASTM C478 SPEC FOR "PRECAST REINFORCED CONCRETE MANHOLE SECTIONS."
 - BUTYL RESIN SECTION JOINT CONFORMS TO LATEST ASTM C443 SPEC.
 - STEEL REINFORCED COPOLYMER POLYPROPYLENE PLASTIC STEP CONFORMS TO LATEST ASTM C478 SPEC.
 - PIPE NOT SUPPLIED.
 - CONE SECTION AVAILABLE IN 4'-0" I.D. ONLY.
 - FOR INLETS LARGER THAN 10" THE DESIGN AND DIMENSION WILL BE DETERMINED FOR EACH PARTICULAR CASE.

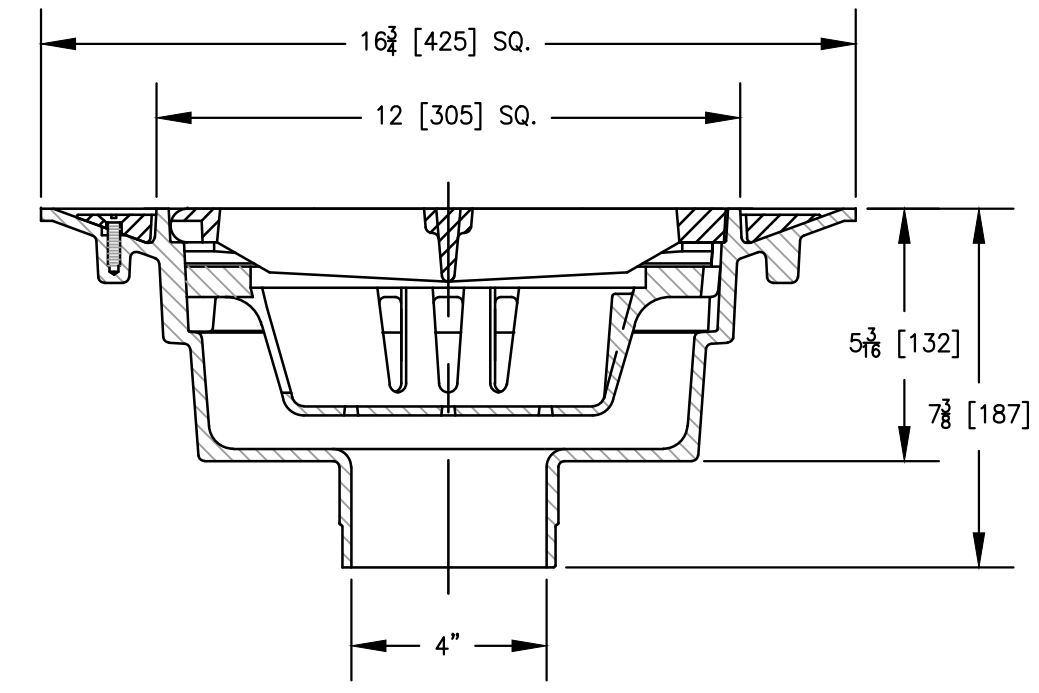
OIL AND GREASE SEPARATOR
 N.T.S.

INLET	D	A	B	W
4"	4'-0"	3'-0"	2'-6"	5"
5"	4'-0"	3'-6"	3'-0"	6"
6"	4'-0"x4'-0"	3'-0"	2'-6"	6"
6"	4'-0"	5'-0"	4'-6"	5"
6"	4'-0"x4'-0"	4'-0"	3'-6"	6"
6"	5'-0"	3'-6"	3'-0"	6"
8"	5'-0"	6'-0"	5'-0"	6"
8"	5'-0"	4'-0"	3'-6"	9"
8"	8'-0"	3'-0"	2'-6"	9"
10"	6'-0"	6'-6"	5'-6"	7"
10"	8'-0"	5'-0"	4'-0"	9"



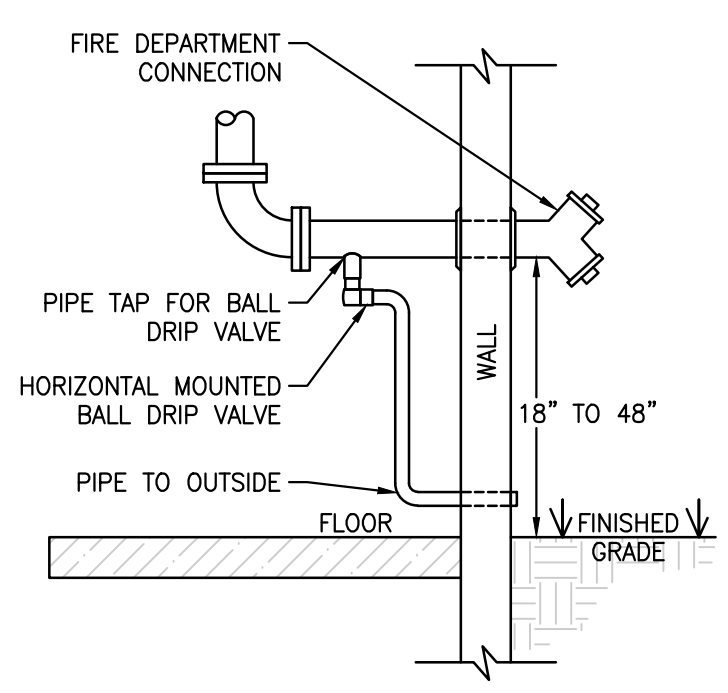
- NOTES:**
- FULL PVC OR IRON SADDLE MAY BE USED TO CONNECT TO EXISTING PVC, CLAY, CONCRETE, OR IRON PIPE. SADDLES MUST HAVE RUBBER GASKETS AND SHALL BE TIGHTENED WITH STRAPS. SADDLES SHALL NOT BE CEMENTED ONTO PIPE.
 - FULL NEW WYE CONNECTION FITTINGS MAY BE SUBSTITUTED FOR SADDLE CONNECTIONS.
 - PIPE SHALL BE CUT TO CONFORM TO THE OPENING IN THE SADDLE.
 - NO CONNECTION TO EXISTING PIPE WITHOUT SADDLE OR FULL NEW WYE FITTING SHALL BE MADE.
 - SHALL BE INSTALLED WITH A DOWNWARD ANGLE AND IN THE WYE FLOW DIRECTION.

TYPICAL SADDLE CONNECTION
 N.T.S.

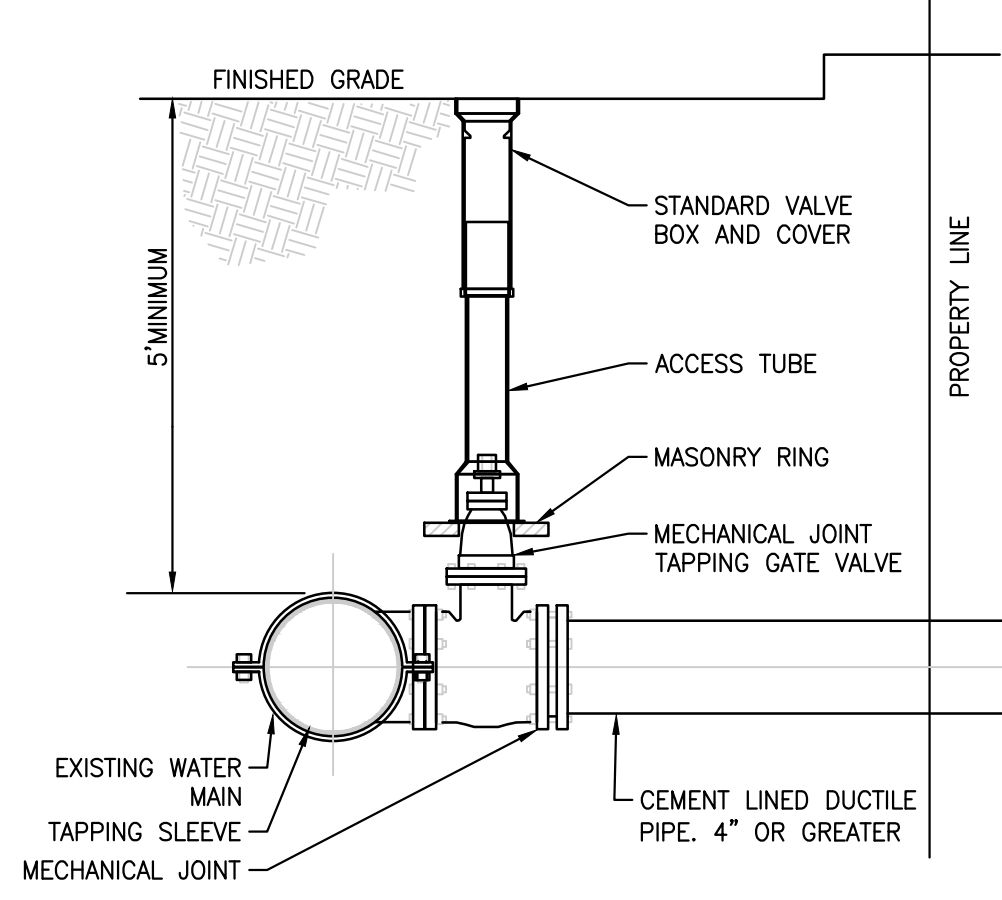


- NOTES:**
- INSTALL PER MANUFACTURER'S SPECIFICATIONS.

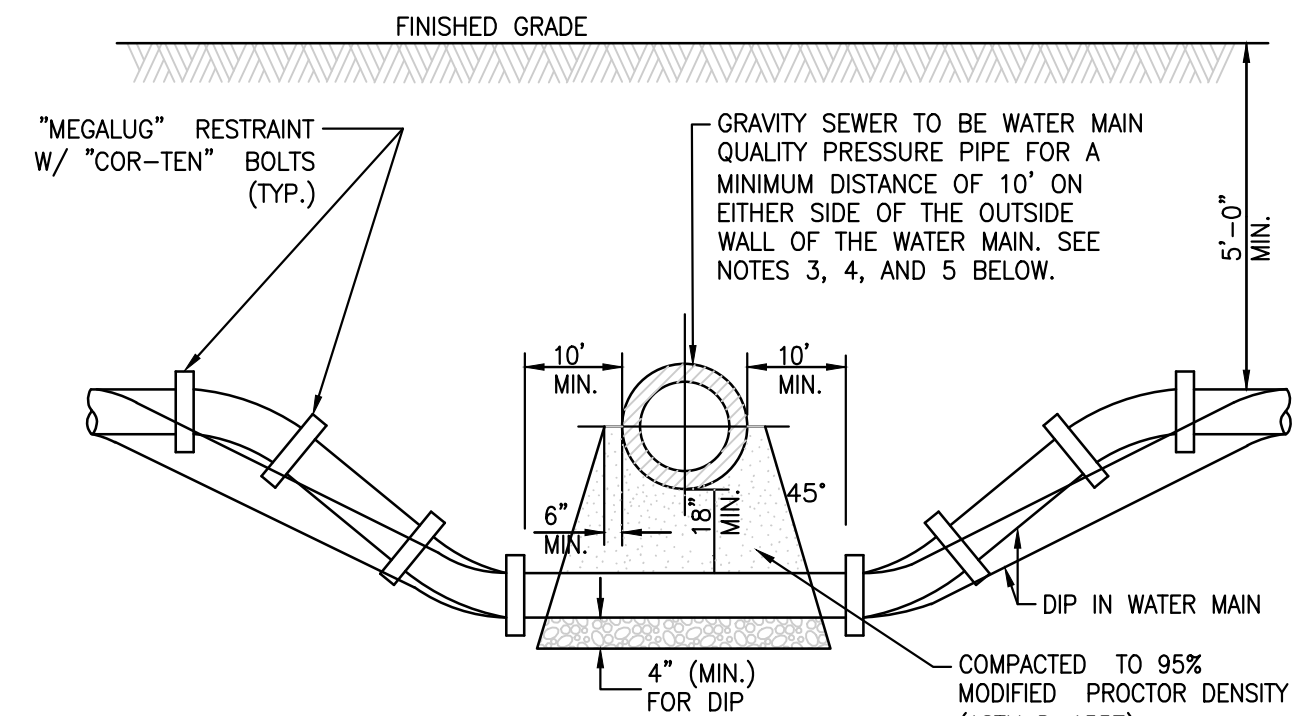
ZURN Z535 FLOOR DRAIN
 N.T.S.



FIRE DEPARTMENT CONNECTION
 N.T.S.

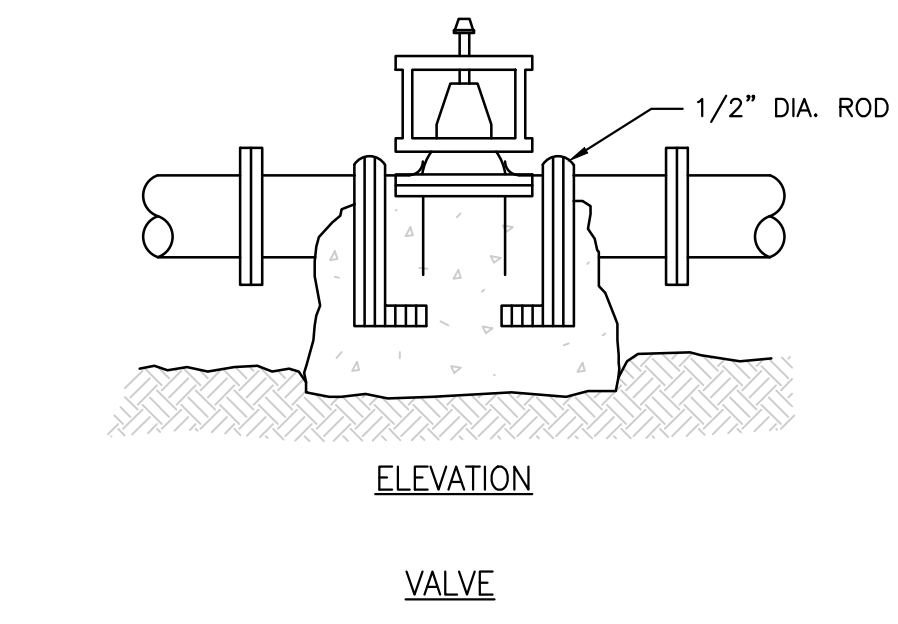
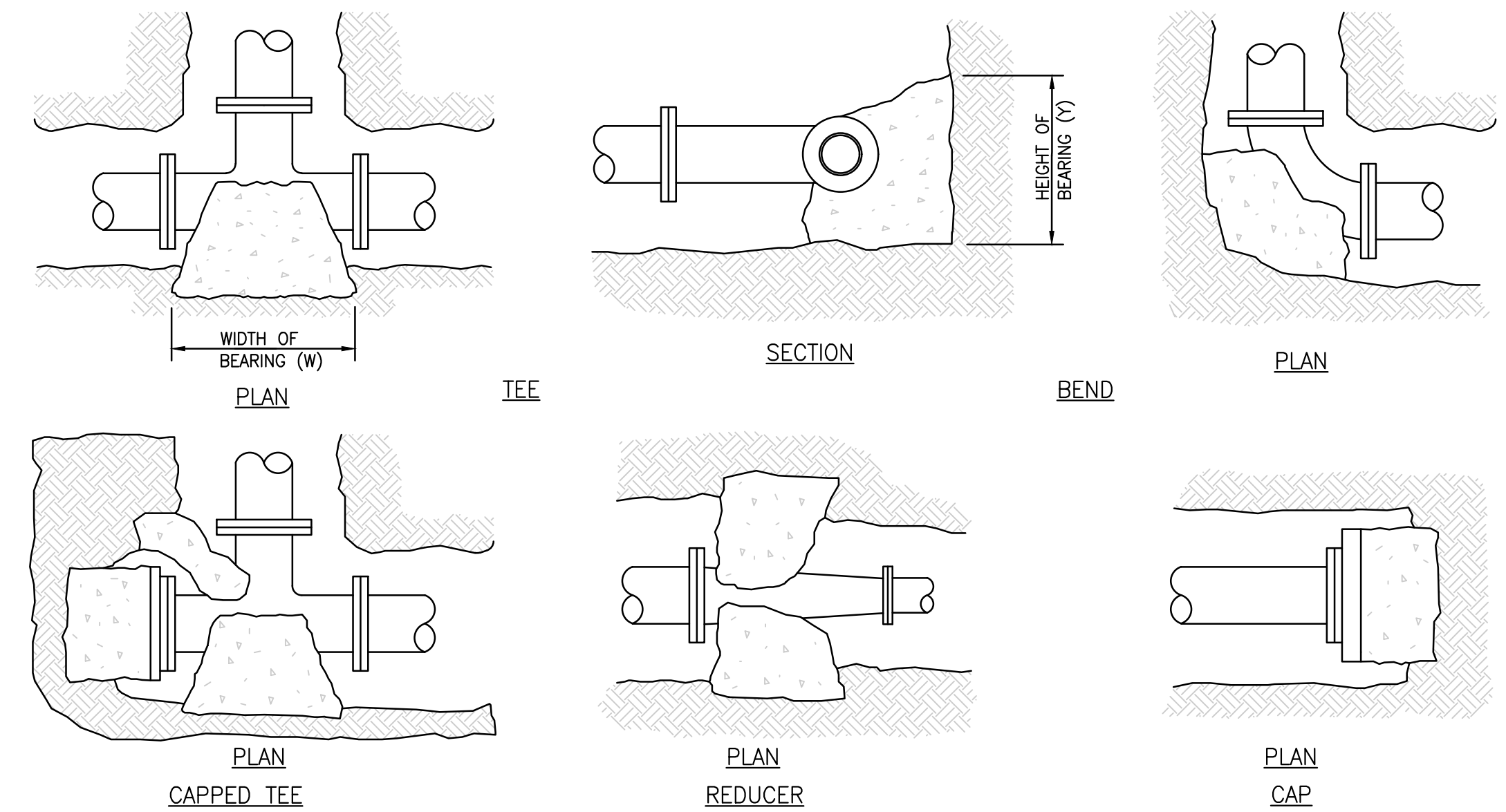


WATER CONNECTION WITH TAPPING SLEEVE AND VALVE
 N.T.S.



- NOTES:**
- HORIZONTAL AND VERTICAL SEPARATION BETWEEN WATERMANS AND SEWERS SHALL COMPLY WITH APPLICABLE SECTIONS OF LOCAL OR STATE REQUIREMENTS, WHICHEVER IS MORE STRINGENT.
 - CONTRACTOR MAY BEND WATER MAIN PIPE UNIFORMLY UNDER SEWERS WITHOUT USING FITTINGS, PROVIDED THAT JOINT DEFLECTION DOES NOT EXCEED 5 DEGREES PER JOINT FOR PIPE UNDER 14" IN DIAMETER AND 3 DEGREES PER JOINT FOR PIPE 14" AND OVER IN DIAMETER. IF FITTINGS ARE USED, CONTINUOUS STRAPPING WITH RODS, STRAPS, NUTS AND BOLTS BELOW NORMAL WATERMAIN DEPTH ARE REQUIRED, OR RETAINER GLANDS MAY BE USED IN LIEU OF STRAPPING. RETAINER GLANDS TO BE CLOW No. F-1058 OR APPROVED EQUAL.
 - ALL SANITARY SEWER (INCLUDING SERVICE) CROSSINGS WHERE THE WATER MAINS OR WATER SERVICES ARE LESS THAN 18" VERTICALLY ABOVE THE SEWER SHALL BE POLYVINYL CHLORIDE PRESSURE PIPE (SDR 26-160 PSI) AND SHALL CONFORM WITH THE LATEST REVISION OF ASTM D-2241. JOINTS SHALL CONFORM TO ASTM D-3139 AND ELASTOMERIC GASKETS SHALL CONFORM TO ASTM F-477. THE SAME PIPE AND JOINT MATERIALS SHALL BE USED WHENEVER WATER MAIN CROSSES BELOW THE SEWER.
 - ALL STORM SEWER (INCLUDING SERVICE) CROSSINGS WHERE THE WATER MAINS ARE LESS THAN 18" VERTICALLY ABOVE THE SEWER SHALL BE REINFORCED CONCRETE PIPE, ASTM C-361, CLASS D-25 WITH BELL AND SPIGOT JOINTS AND RUBBER GASKETS, OR PVC SDR 26 AS SPECIFIED IN NOTE 3 ABOVE. THE SAME PIPE AND JOINT MATERIAL SHALL BE USED WHENEVER WATER MAIN CROSSES BELOW THE SEWER.
 - FOR NEW SEWER INSTALLATIONS CROSSING OVER WATER MAINS, THE ENTIRE RUN OF NEW SEWER SHALL BE WATER MAIN QUALITY PIPE, EXTENDING FROM STRUCTURE TO STRUCTURE ON EACH SIDE OF THE CROSSING.

WATER MAIN CROSSING
 N.T.S.



TRUST BLOCKS FOR WATER SYSTEM
 N.T.S.

PIPE SIZE	WATER PIPE	
	TEE, DEAD END, 90° BEND	45° & 22-8° BENDS
4" OR LESS	3 SQ. FEET	3 SQ. FEET
6"	4 SQ. FEET	3 SQ. FEET
8"	6 SQ. FEET	3 SQ. FEET
10"	9 SQ. FEET	5 SQ. FEET
12"	13 SQ. FEET	7 SQ. FEET
16"	23 SQ. FEET	12 SQ. FEET

- NOTES:**
- THRUST BLOCKS TO EXTEND TO UNDISTURBED GROUND.
 - ALL CONCRETE SHALL BE CLASS B.
 - TABLE IS BASED ON 3000 LB./SQ. FT. SOIL. IF SOIL CONDITIONS ARE FOUND TO INDICATE SOIL BEARING LESS, THE AREAS SHALL BE INCREASED ACCORDINGLY.
 - AREAS FOR PIPES GREATER THAN 16" SHALL BE CALCULATED FOR EACH PROJECT.
 - FOR ALL NON BEARING VERTICAL SURFACES.

COPYRIGHT NOTICE
 This drawing is property of Fodera Engineering and is not to be used for any purpose other than the specific project and site named herein, and cannot be reproduced in any manner without the express written permission from the engineer on record.

REVISION	DATE	BY
REV-12 UNITS	2/1/22	GGF
DRIVE PATH REV.	4/6/22	GGF
ENG. COMMENTS	6/1/22	GGF
CPDC REVISION	9/7/22	GGF

PROJECT LOCATION:
 459 MAIN STREET
 READING, MA 01867
PARCEL ID:
 MAP 17, PARCEL 62

PLAN SET:
 PROPOSED 40R DEVELOPMENT
 459 MAIN STREET
 READING, MA 01867

DECEMBER 1, 2021

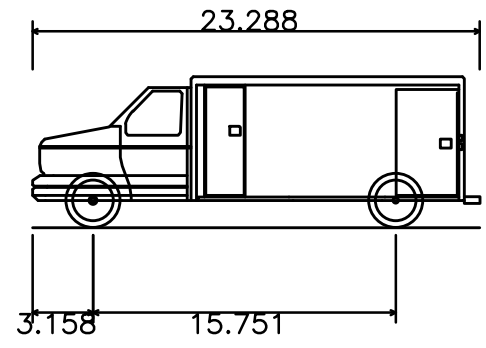
SCALE: 1" = 10'



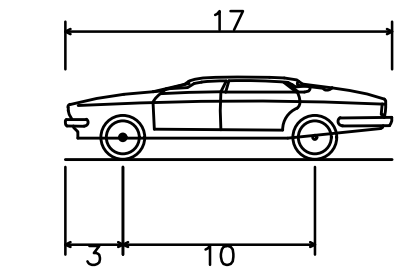
JOB NO.: 21228-238

SHEET TITLE:
 VEHICLE TRACKING

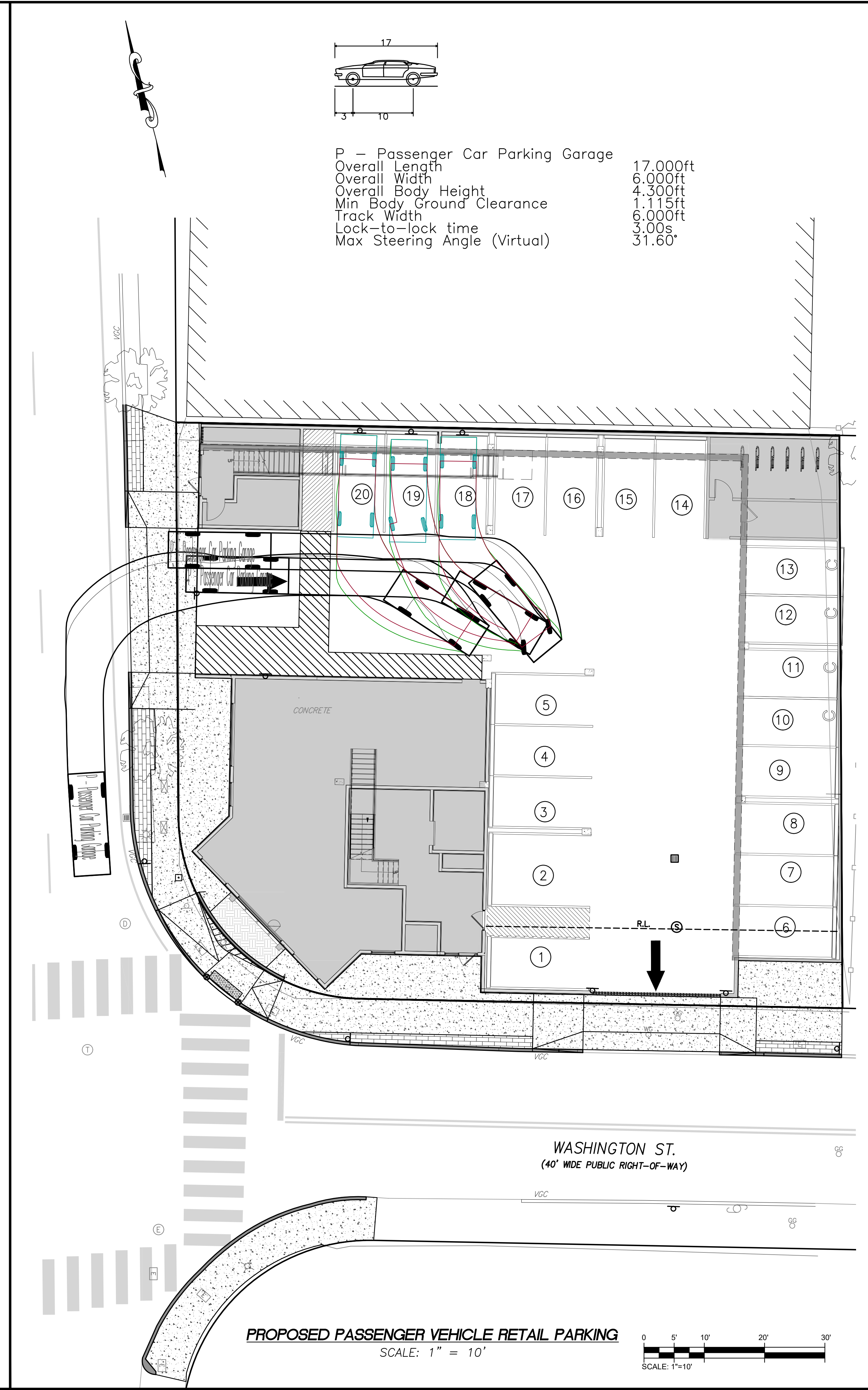
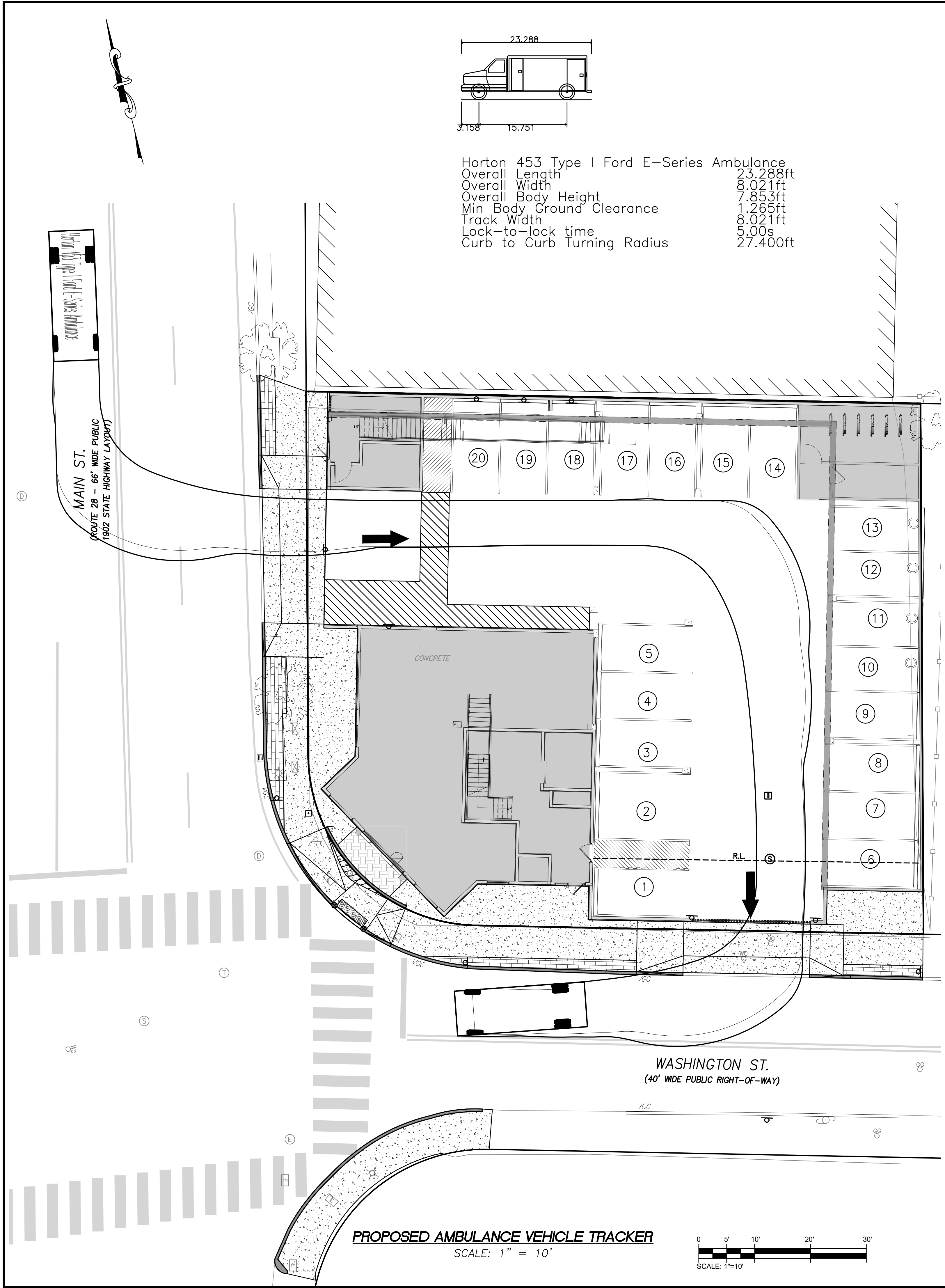
SHEET NUMBER:
 C-6

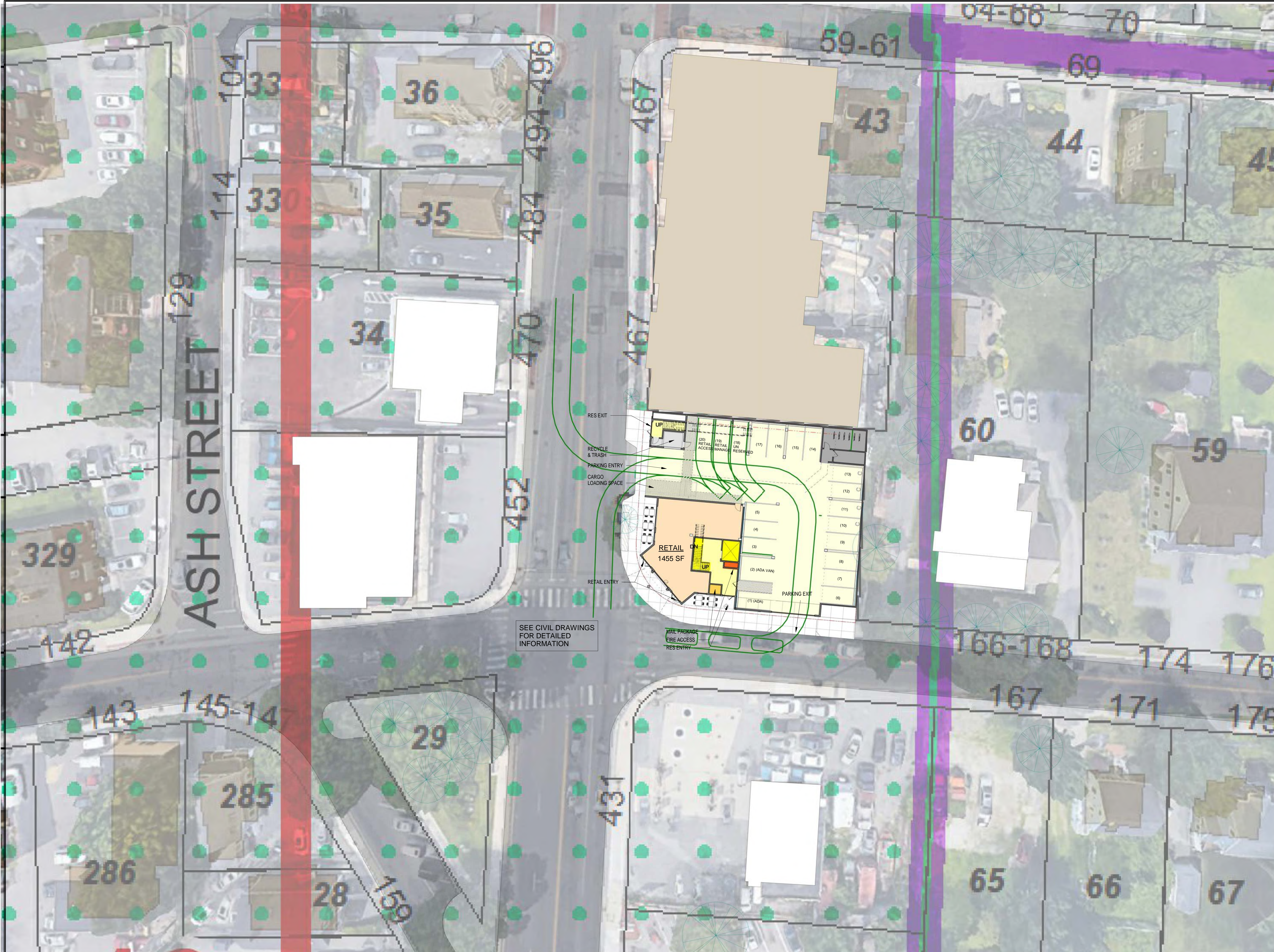


Horton 453 Type I Ford E-Series Ambulance
 Overall Length 23.288ft
 Overall Width 8.021ft
 Overall Body Height 7.853ft
 Min Body Ground Clearance 1.265ft
 Track Width 8.021ft
 Lock-to-lock time 5.00s
 Curb to Curb Turning Radius 27.400ft



P - Passenger Car Parking Garage
 Overall Length 17.000ft
 Overall Width 6.000ft
 Overall Body Height 4.300ft
 Min Body Ground Clearance 1.115ft
 Track Width 6.000ft
 Lock-to-lock time 3.00s
 Max Steering Angle (Virtual) 31.60°





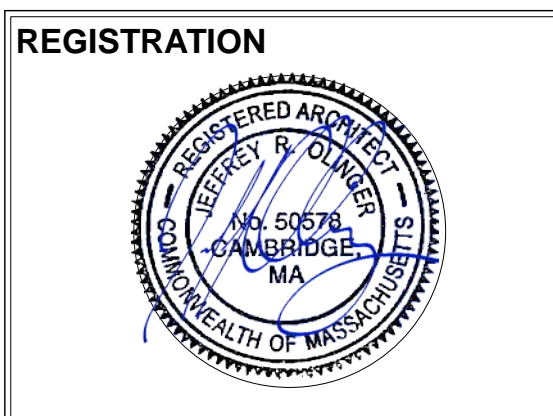
PROJECT NAME
459 MAIN STREET REDEVELOPMENT

PROJECT ADDRESS:
 459 MAIN STREET, READING, MA 01867

APPLICANT
 Giuseppe C. Fodera
 GCfodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 1" = 20'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022

SITE PLAN

A001
 459 MAIN STREET

PROJECT NAME
459 MAIN STREET REDEVELOPMENT

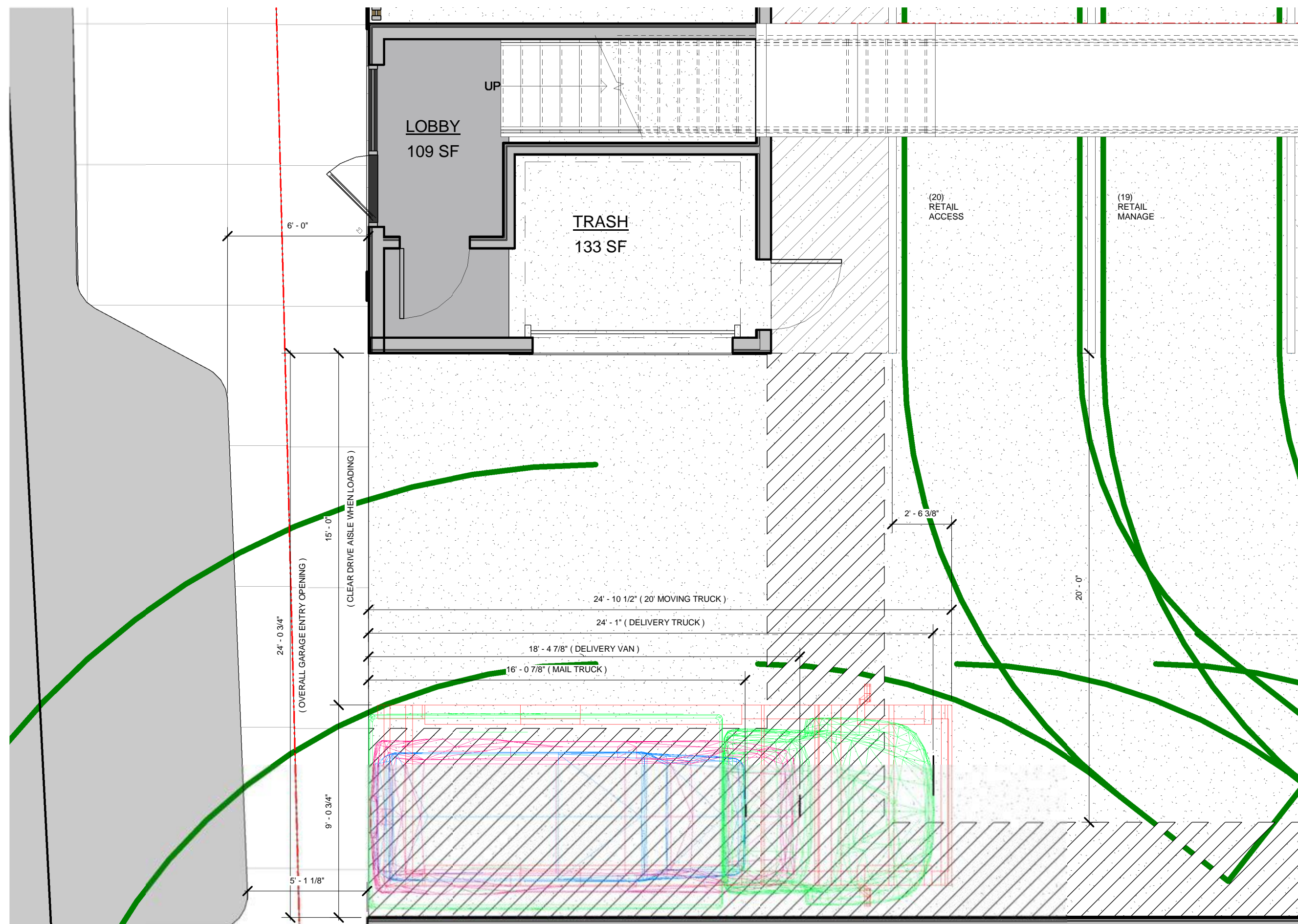
PROJECT ADDRESS:
 459 MAIN STREET, READING, MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

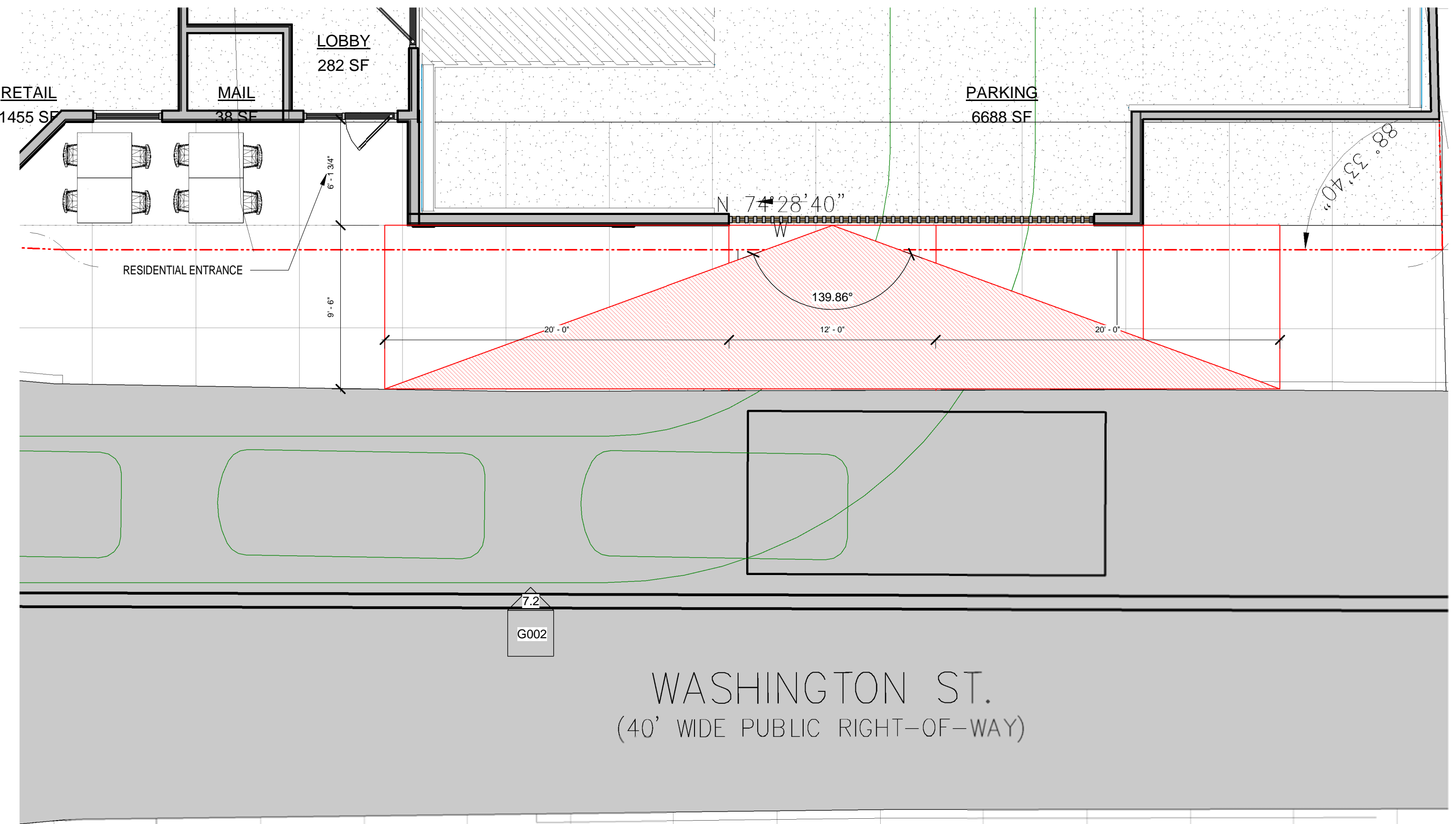
ARCHITECT
Olinger Architects

77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

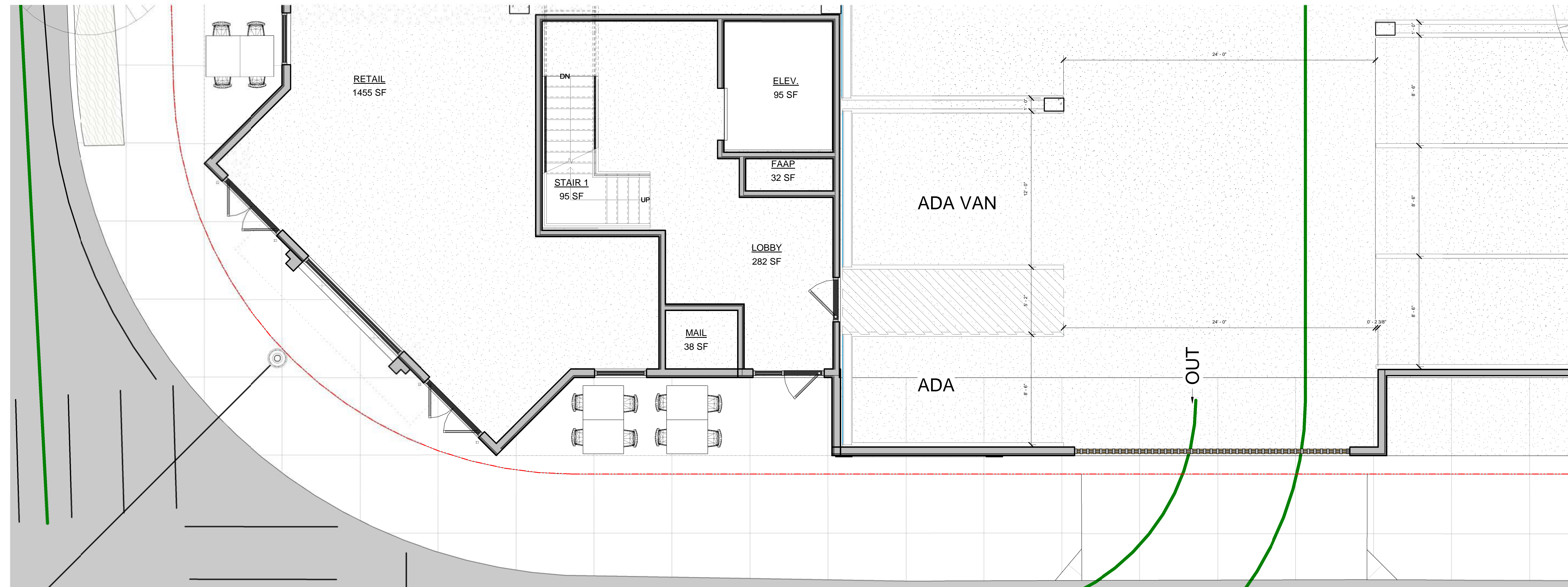
CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



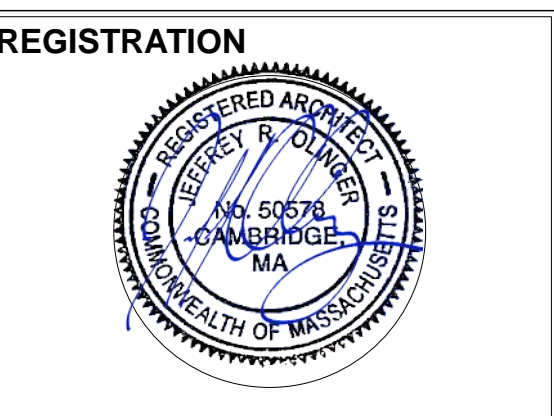
1 A002 EXTERIOR AREA DETAIL PLAN 1
 1/4" = 1'-0"



3 A002 EXTERIOR AREA PARKING ENTRANCE TURNING
 3/16" = 1'-0"



2 A002 EXTERIOR AREA DETAIL PLAN 2
 1/4" = 1'-0"



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	As indicated

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022
4	ZONING UPDATE	05/31/2022

EXTERIOR AREA DETAIL PLANS

A002
 459 MAIN STREET

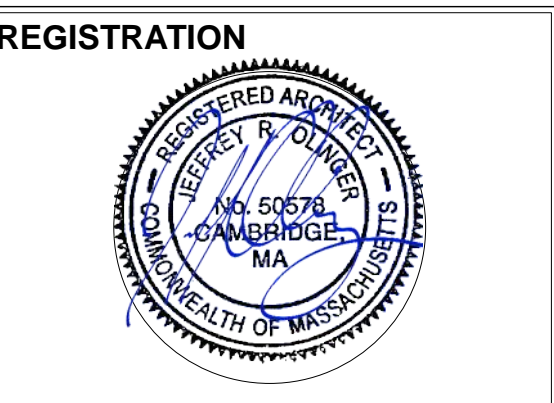
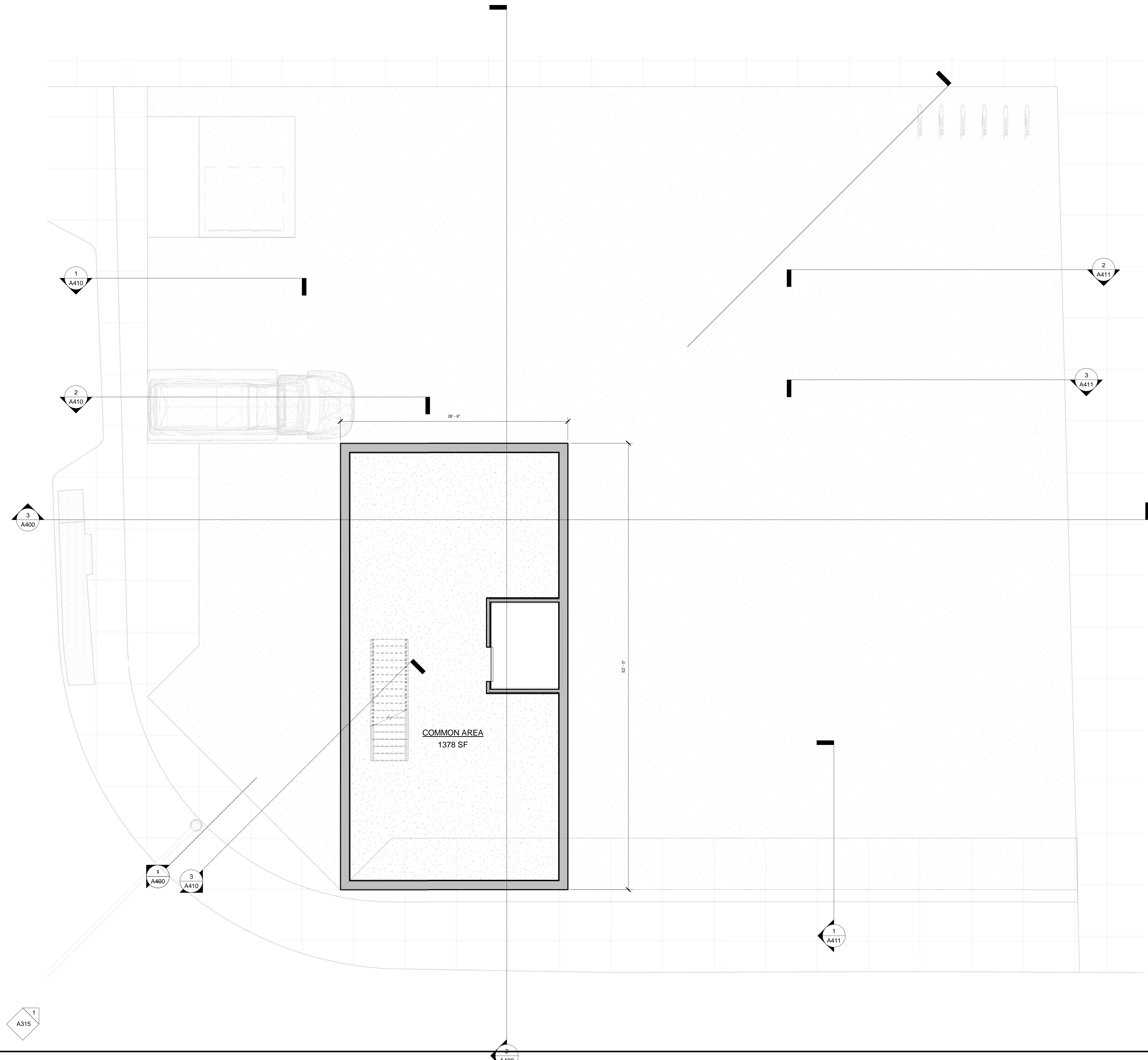
PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GCFodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	3/16" = 1'-0"

REVISIONS

No.	Description	Date
2	ZONING UPDATE	03/07/2022

BASEMENT PLAN

A100
 459 MAIN STREET

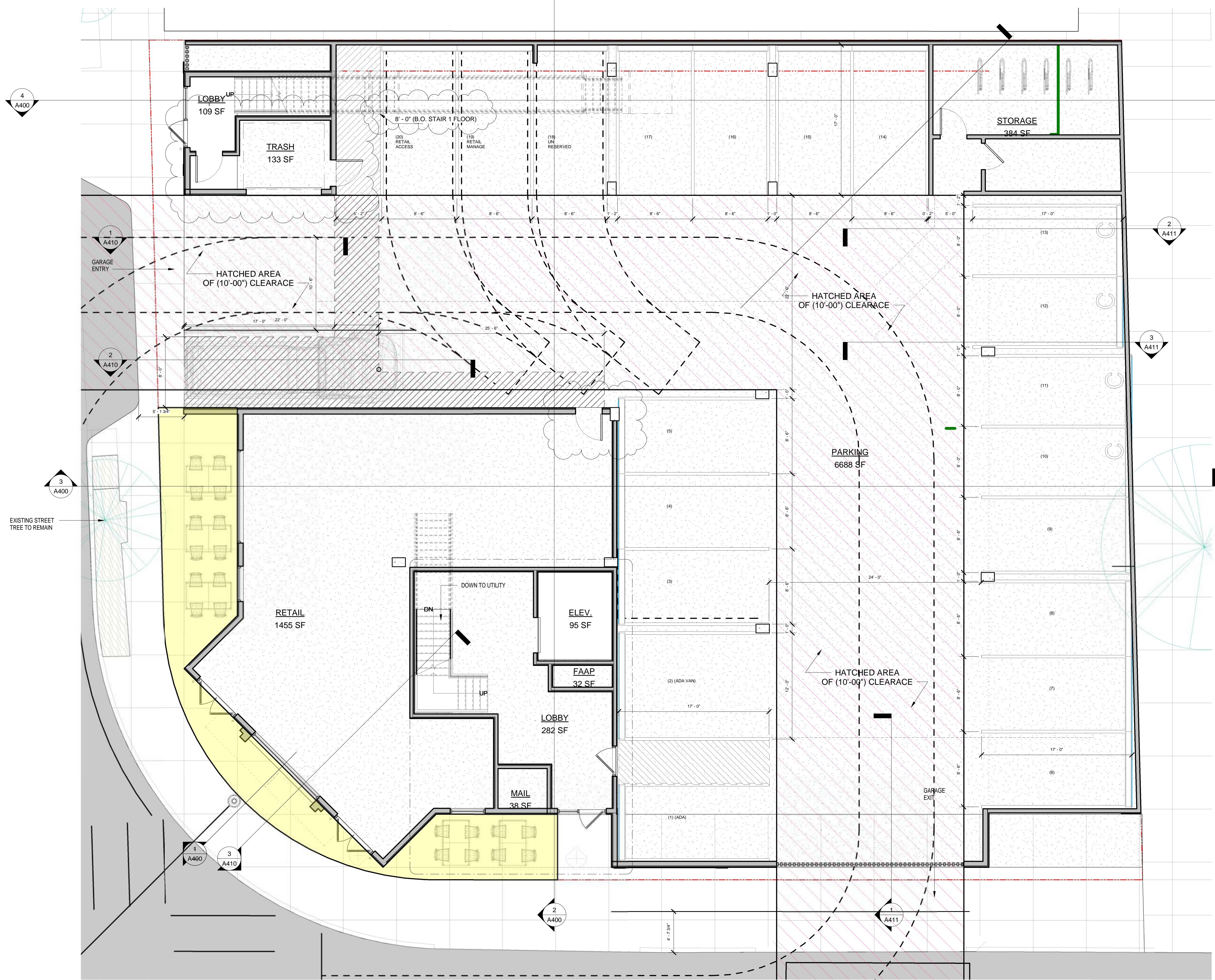
PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

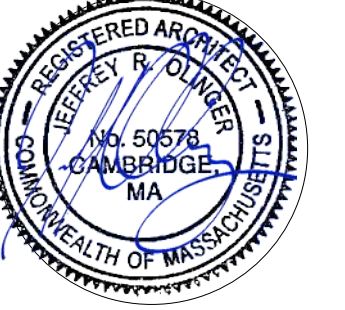
APPLICANT
 Giuseppe C. Fodera
 GCFodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 3/16" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022
3	ZONING UPDATE	04/05/2022
4	ZONING UPDATE	05/31/2022
5	ZONING UPDATE	09/07/2022

LEVEL 01 PLAN

A101

459 MAIN STREET

1
A315

PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

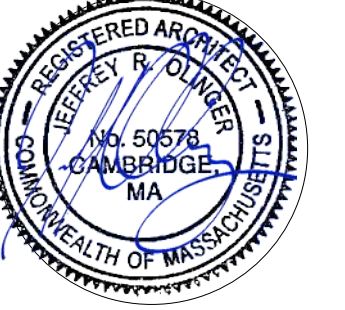
APPLICANT
 Giuseppe C. Fodera
 GCFodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 3/16" = 1'-0"

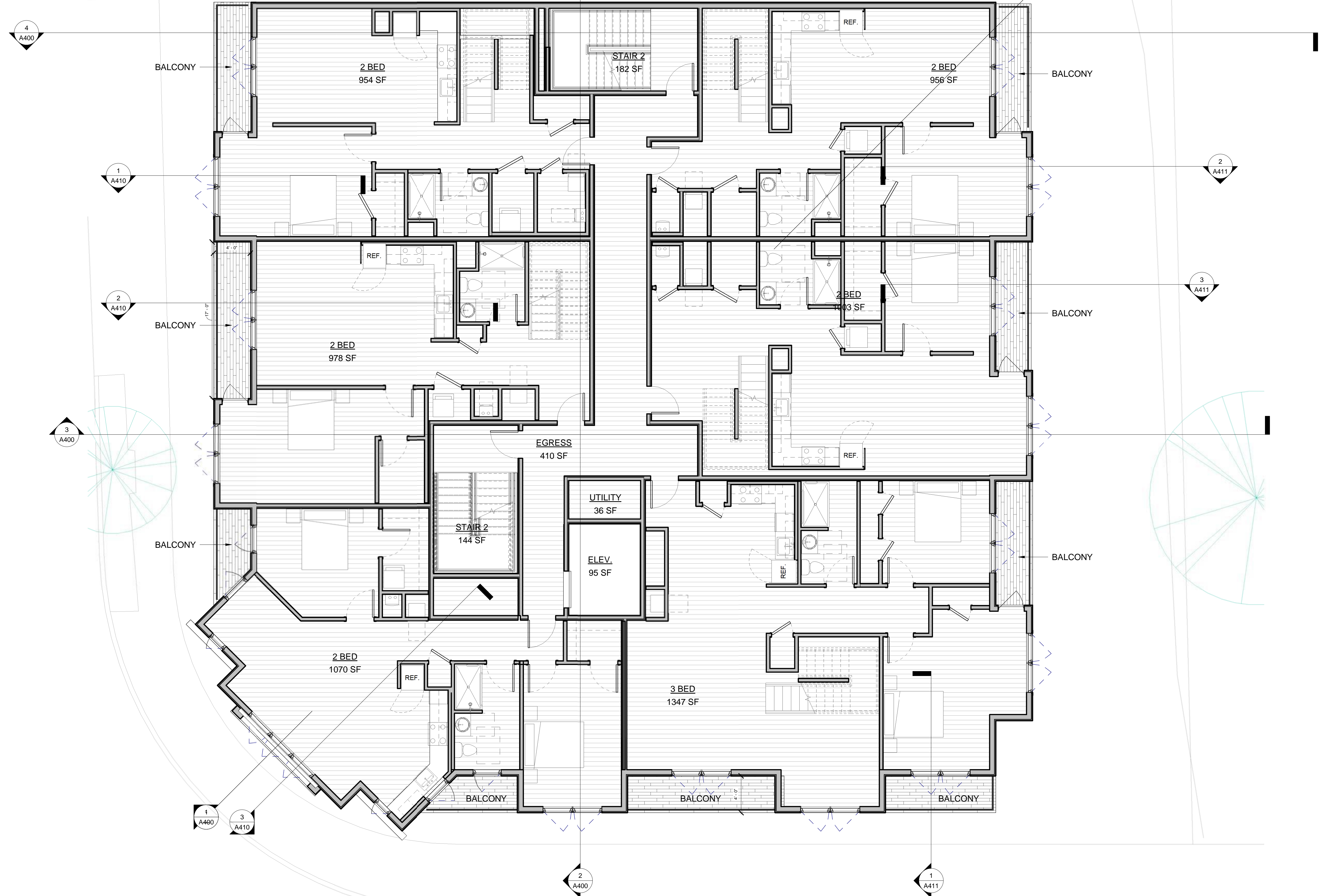
REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022
3	ZONING UPDATE	04/05/2022
4	ZONING UPDATE	05/31/2022

LEVEL 02 PLAN

A102

459 MAIN STREET



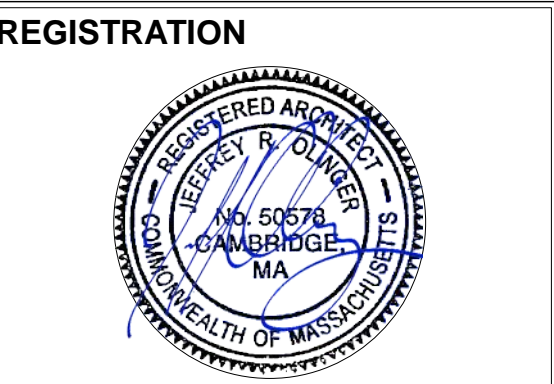
PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GCFodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	3/16" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

LEVEL 03 PLAN

A103
 459 MAIN STREET

1
 A315

4
A400

1
A410

2
A410

3
A400

1
A490

3
A410

2
A400

1
A411

2
A411

3
A411

1
A315



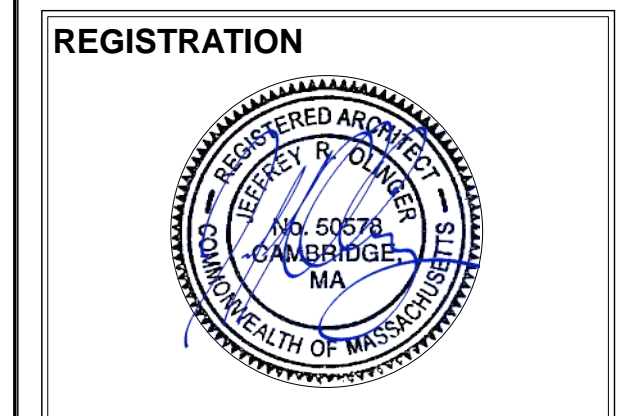
PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



ZONING REVIEW

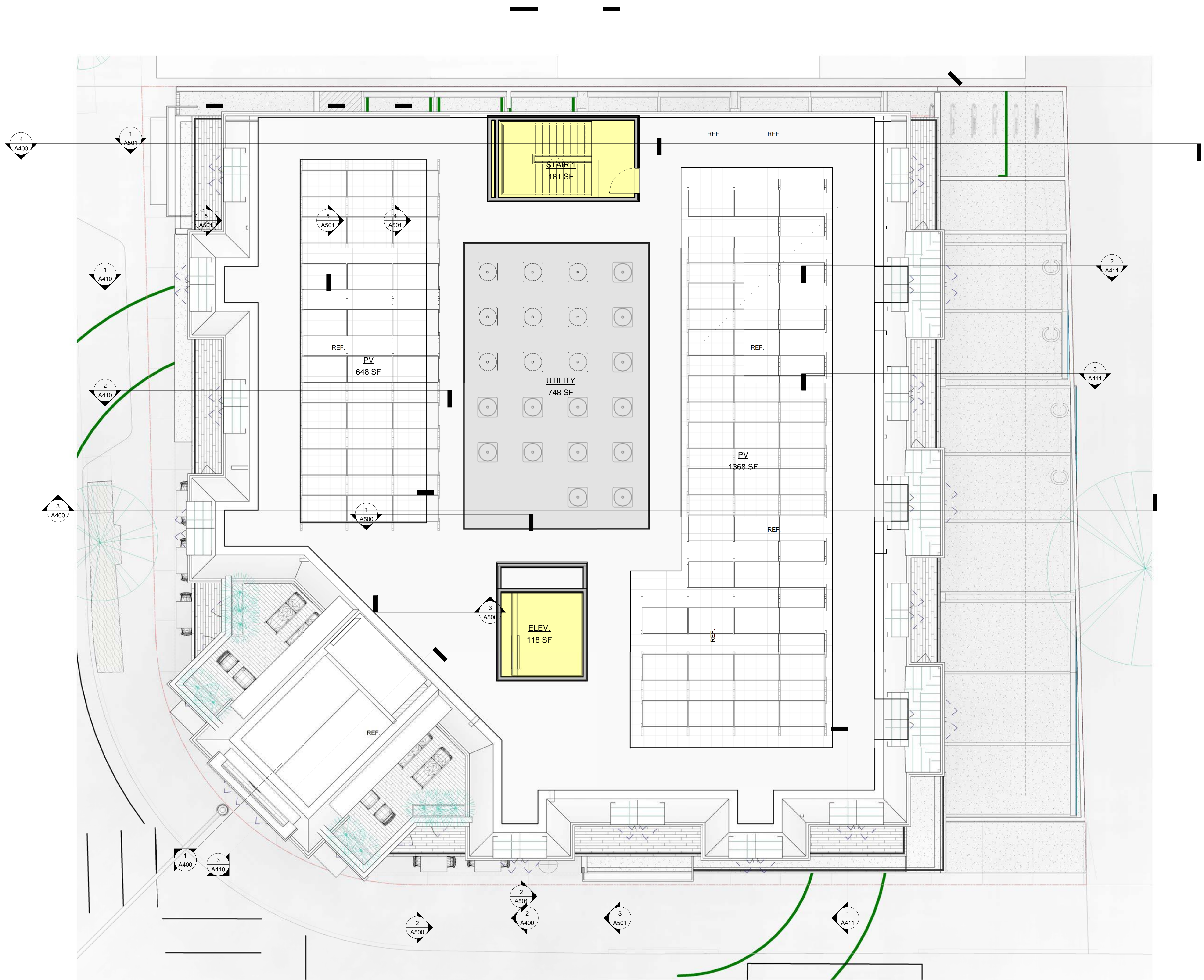
Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	3/16" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

LEVEL 04 PLAN

A104
 459 MAIN STREET



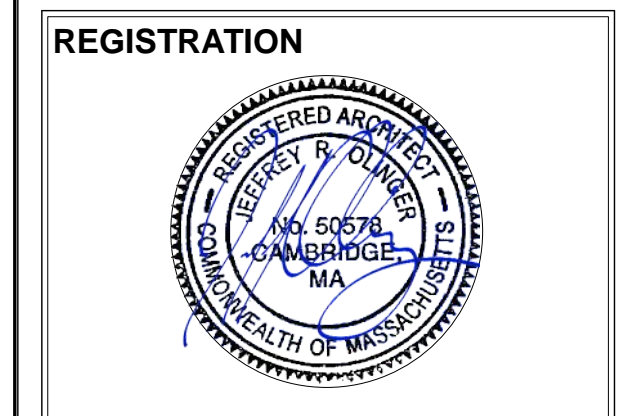
PROJECT NAME
459 MAIN STREET REDEVELOPMENT

PROJECT ADDRESS:
 459 MAIN STREET, READING, MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	3/16" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

ROOF PLAN

A105
 459 MAIN STREET

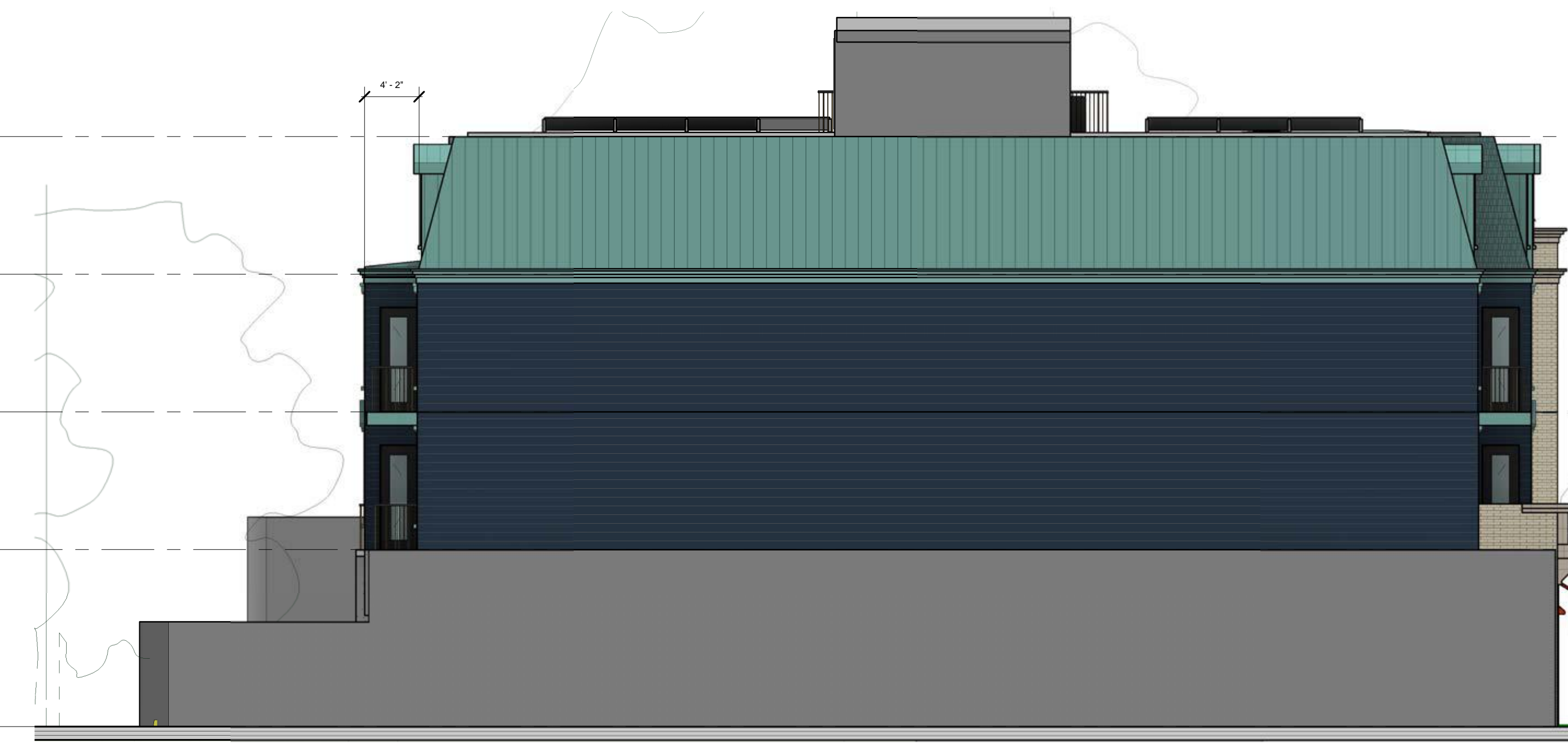
PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



3 A300_COMPOSITE ELEVATION_NORTH
 1/8" = 1'-0"



2 A300_COMPOSITE ELEVATION_WEST
 1/8" = 1'-0"



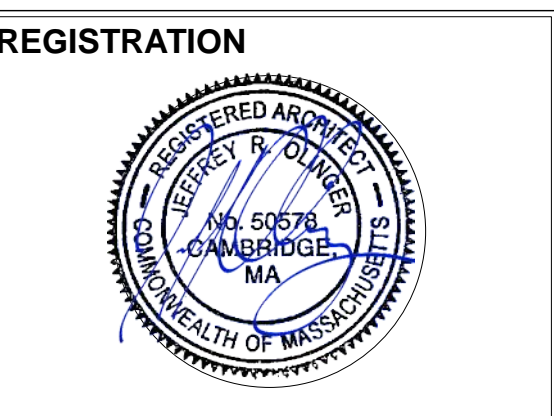
5 A300_COMPOSITE ELEVATION_SOUTHWEST CORNER
 1/8" = 1'-0"



4 A300_COMPOSITE ELEVATION_SOUTH
 1/8" = 1'-0"



1 A300_COMPOSITE ELEVATION_EAST
 1/8" = 1'-0"



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	1/8" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022
3	ZONING UPDATE	04/05/2022
4	ZONING UPDATE	05/31/2022

**COMPOSITE
 ELEVATIONS**

A300
 459 MAIN STREET

PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

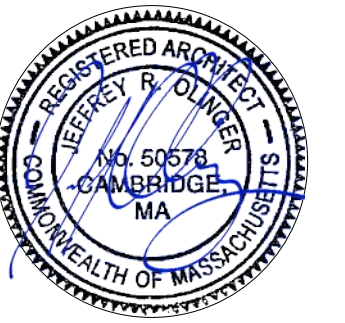
ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492

MATERIAL NOTES ARE TYPICAL,
 SEE (G006) FOR PROJECT MATERIALS



REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022

**WEST BUILDING
 ELEVATION**

A310

459 MAIN STREET



PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

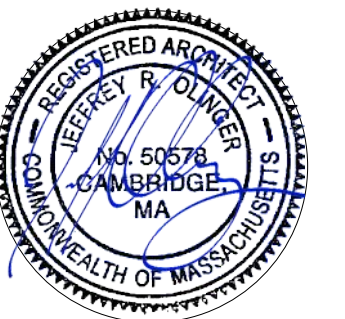
PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492

REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022
3	ZONING UPDATE	04/05/2022
4	ZONING UPDATE	05/31/2022

**SOUTH BUILDING
 ELEVATION**

A312

459 MAIN STREET

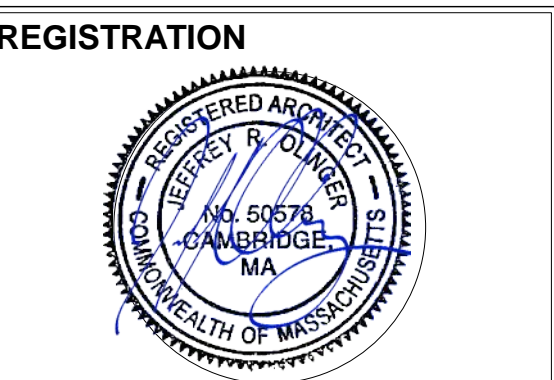
PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
3	ZONING UPDATE	04/05/2022
4	ZONING UPDATE	05/31/2022

**EAST BUILDING
 ELEVATION**

A313
 459 MAIN STREET

PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GCFodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

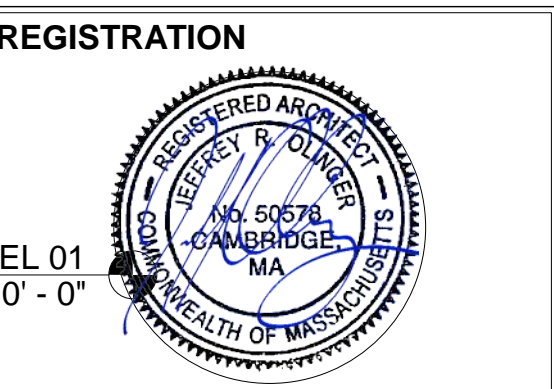
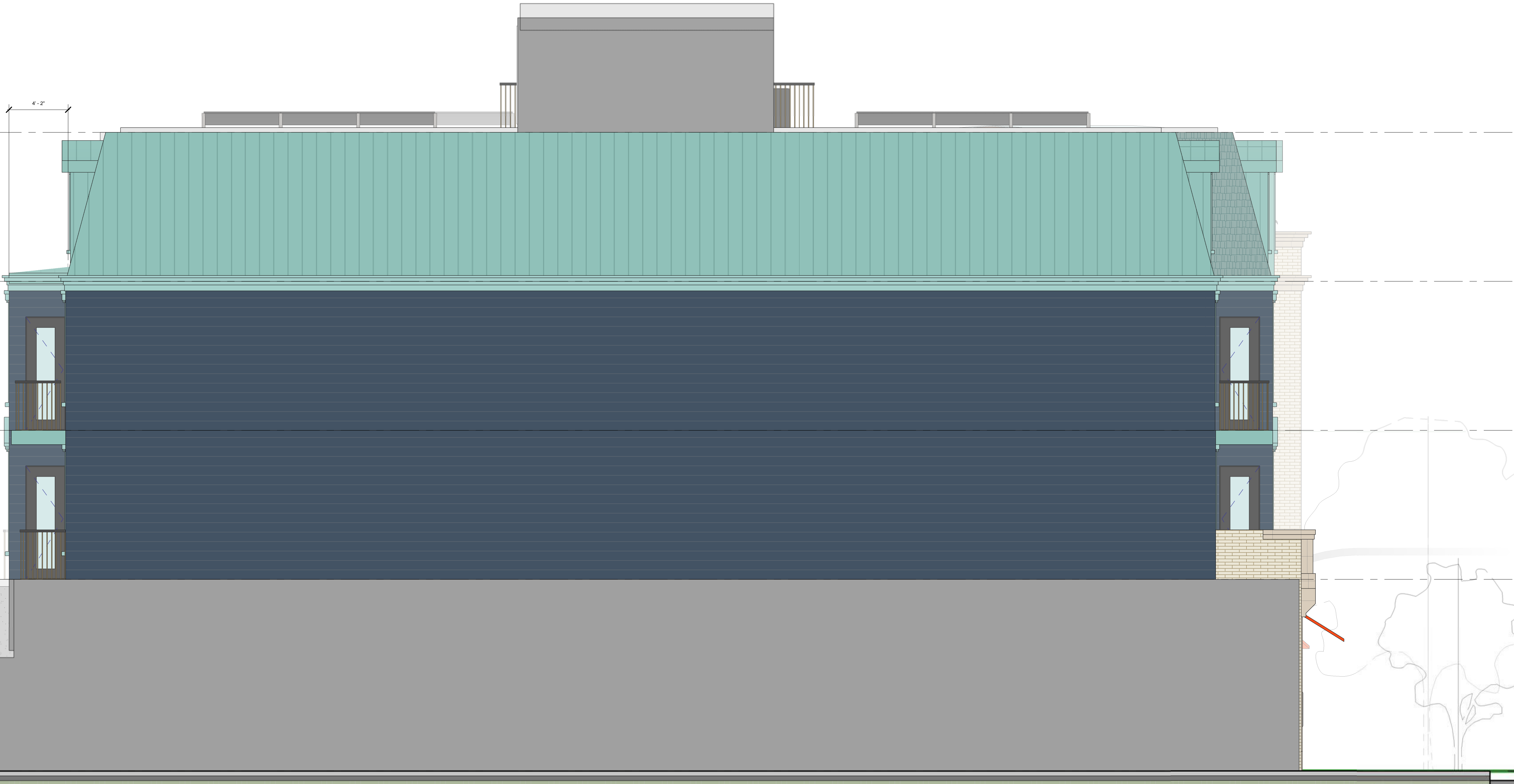
ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

ROOF
 45' - 0" CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492

LEVEL 04
 34' - 6"

LEVEL 03
 24' - 0"

LEVEL 01
 0' - 0"



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
3	ZONING UPDATE	04/05/2022

**NORTH BUILDING
 ELEVATION**

A314
 459 MAIN STREET

PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

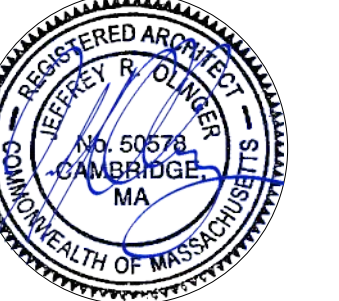
ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



1 A315_SOUTHWEST CORNER & MISC.
 1/4" = 1'-0"

REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

**CORNER & MISC.
 ELEVATIONS**

A315
 459 MAIN STREET

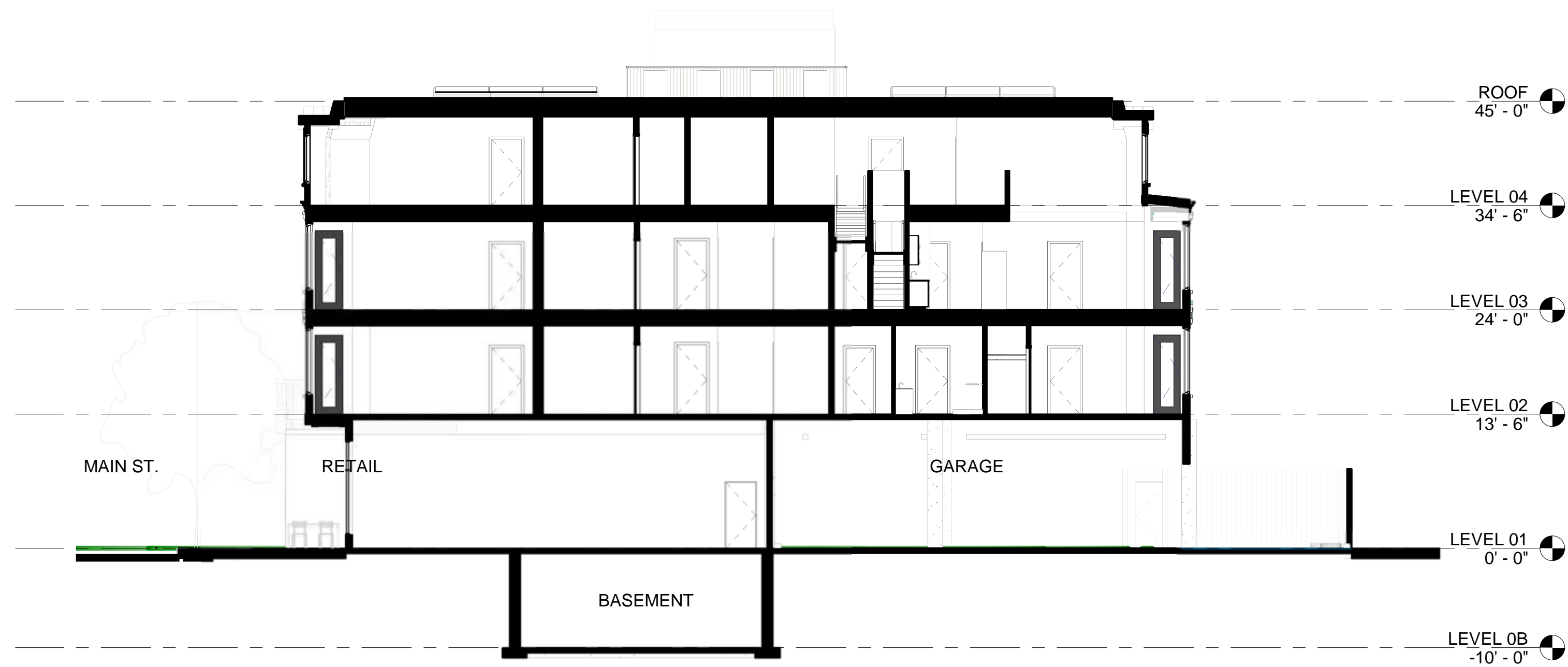
PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GCFodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

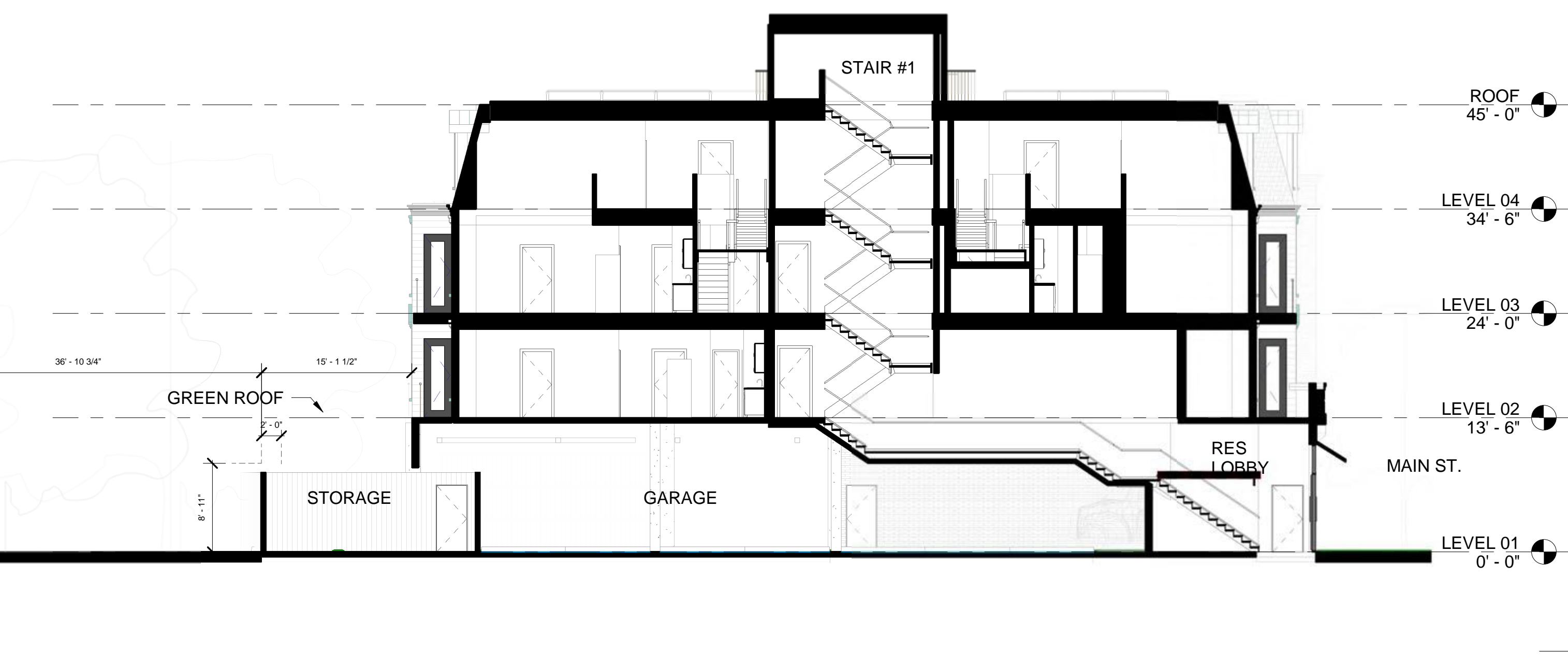
CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



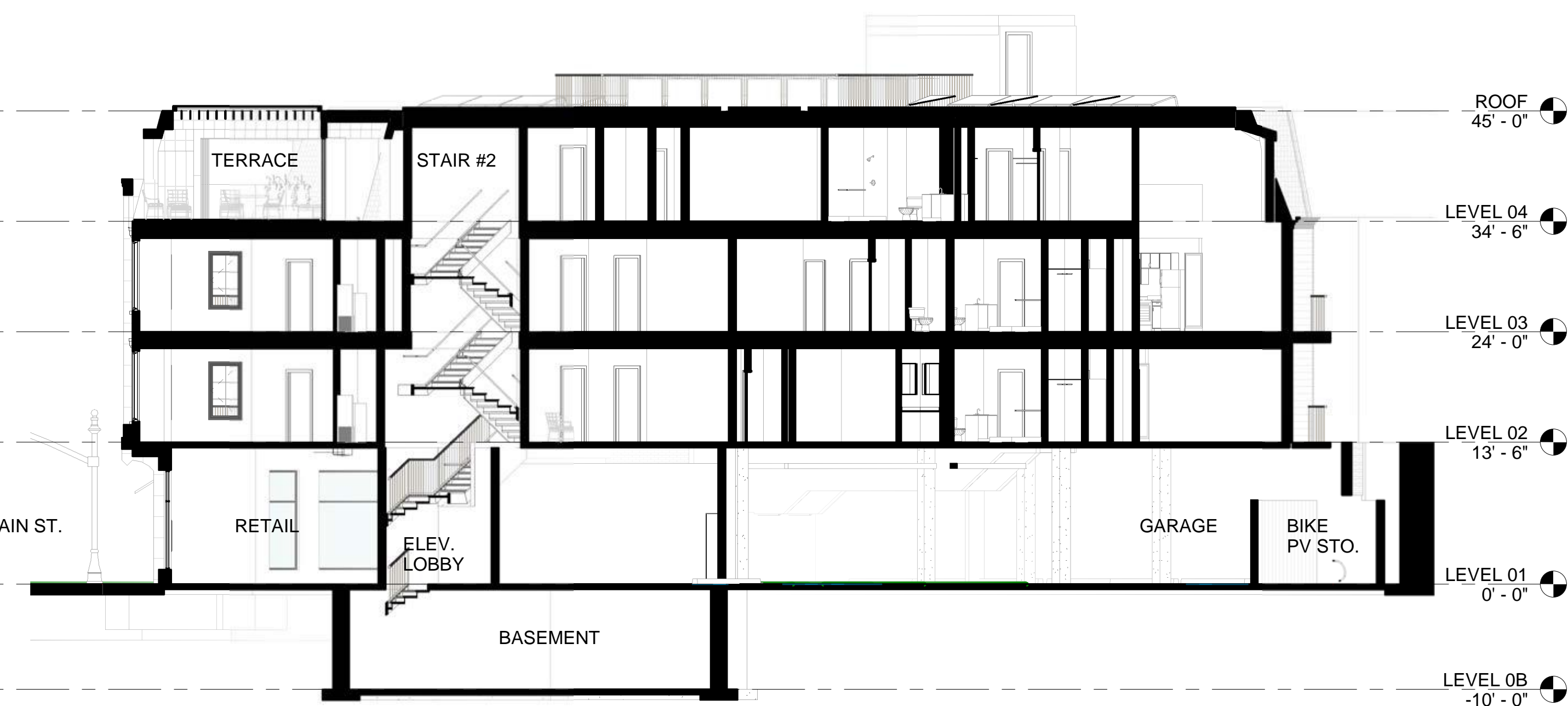
3 A400 WEST-EAST SECTION
 1" = 10'-0"



2 A400 NORTH-SOUTH SECTION
 1" = 10'-0"

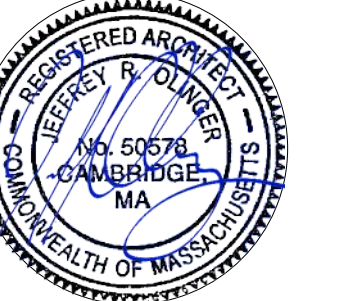


4 A400 WEST-EAST SECTION @ STAIR
 1" = 10'-0"



1 A400 CORNER SECTION
 1" = 10'-0"

REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 1" = 10'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
2	ZONING UPDATE	03/07/2022
3	ZONING UPDATE	04/05/2022

BUILDING SECTIONS

A400

459 MAIN STREET

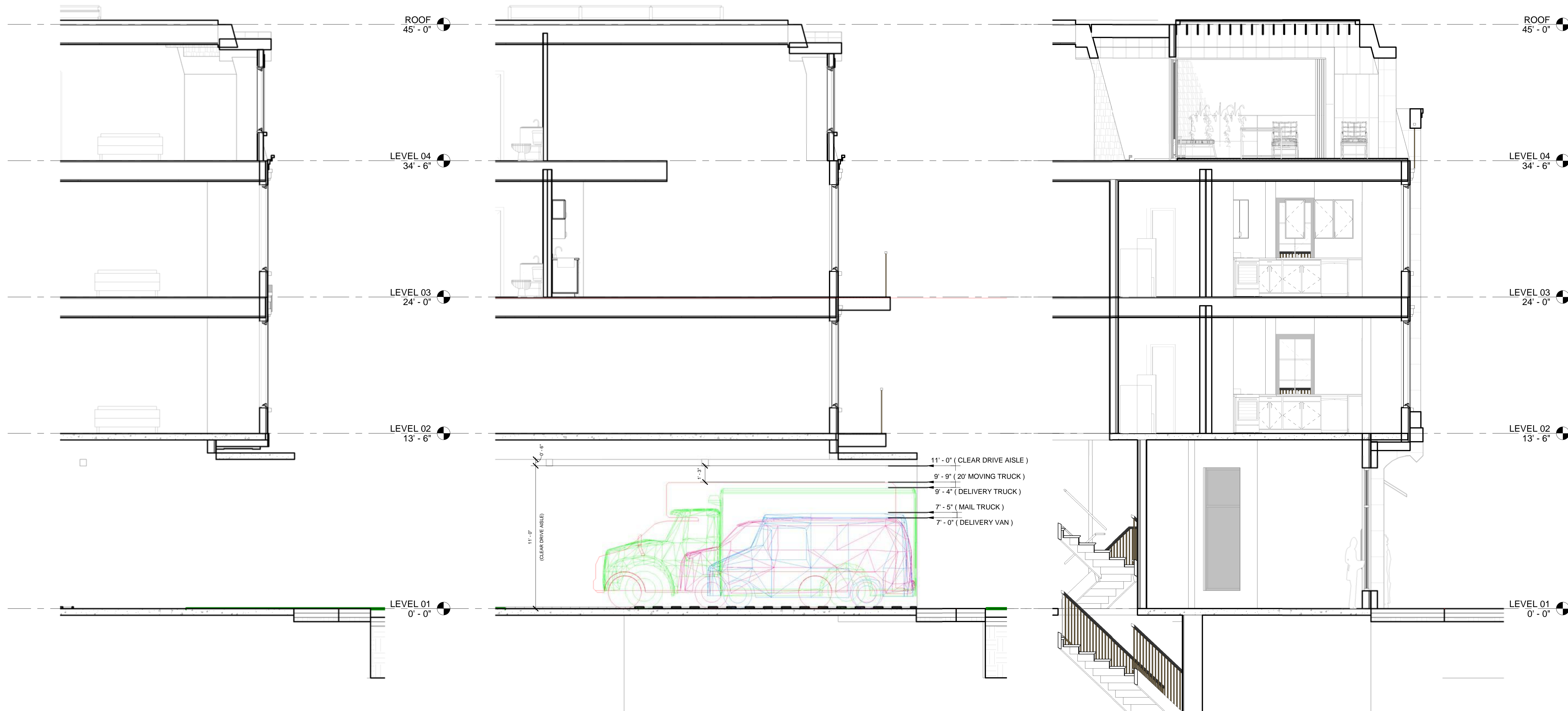
PROJECT NAME
459 MAIN STREET REDEVELOPMENT

PROJECT ADDRESS:
 459 MAIN STREET, READING, MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

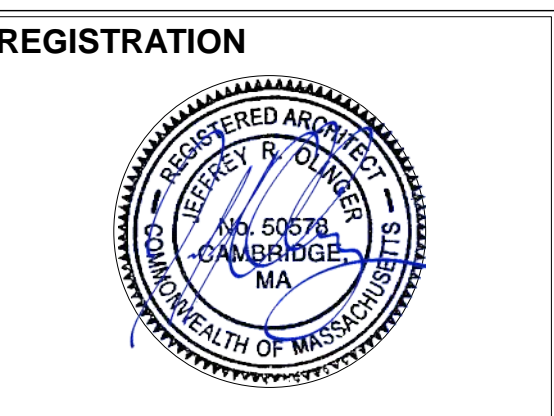
CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



1 A410_MAIN WALL SECTION 01
 1/4" = 1'-0"

2 A410_MAIN WALL SECTION 2
 1/4" = 1'-0"

3 A410_CORNER WALL SECTION
 1/4" = 1'-0"



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
4	ZONING UPDATE	05/31/2022

WALL SECTIONS 1

A410
 459 MAIN STREET

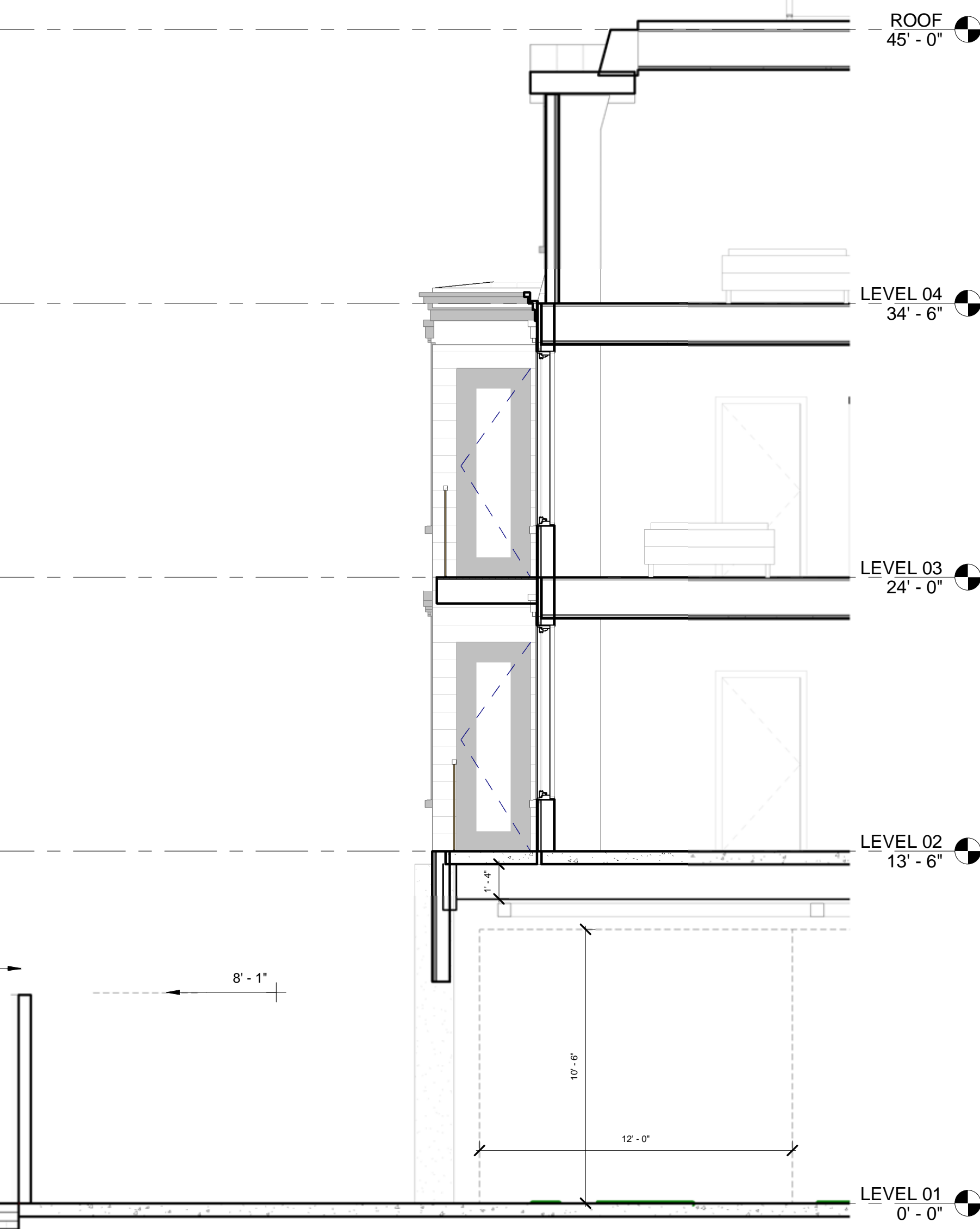
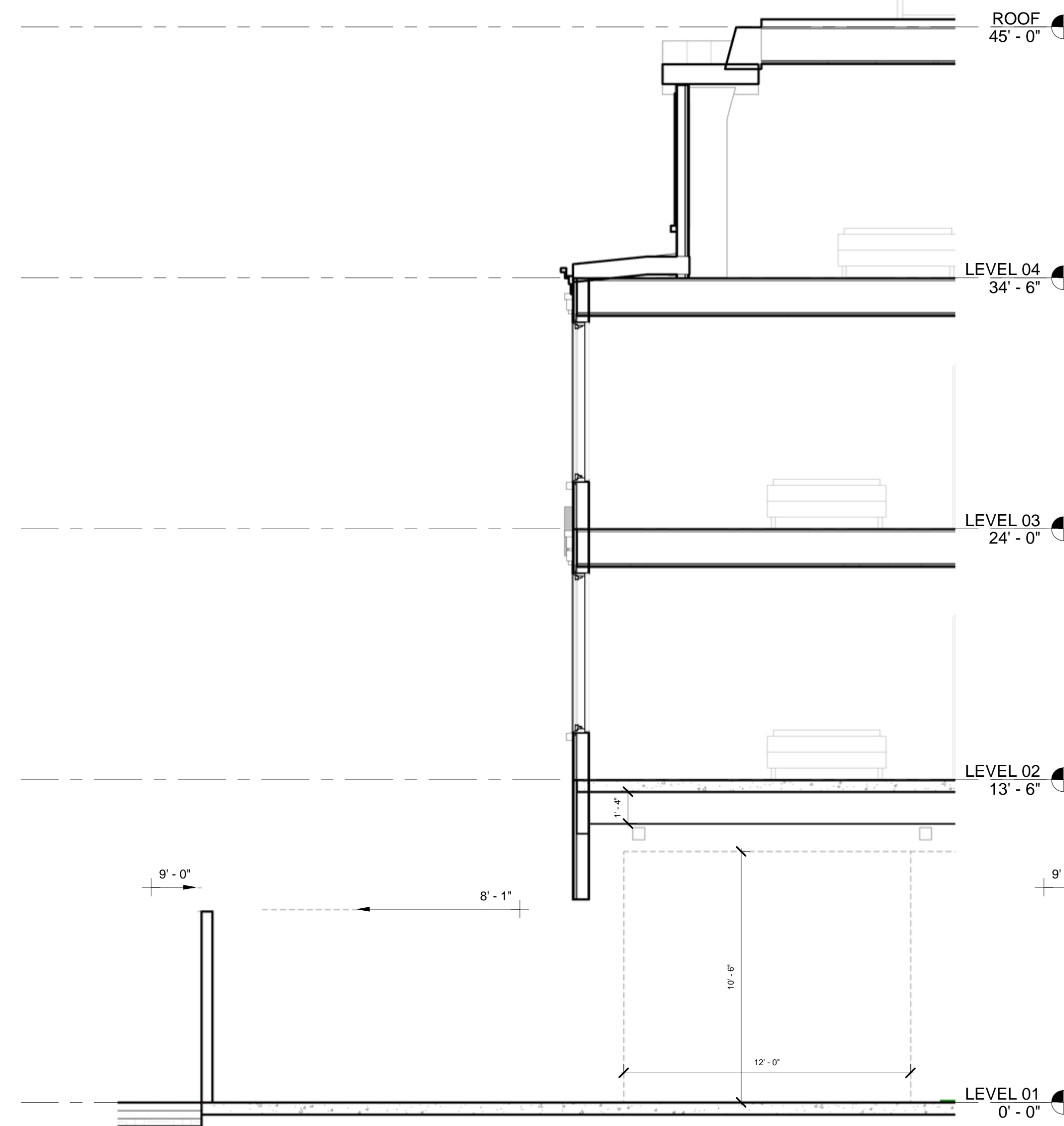
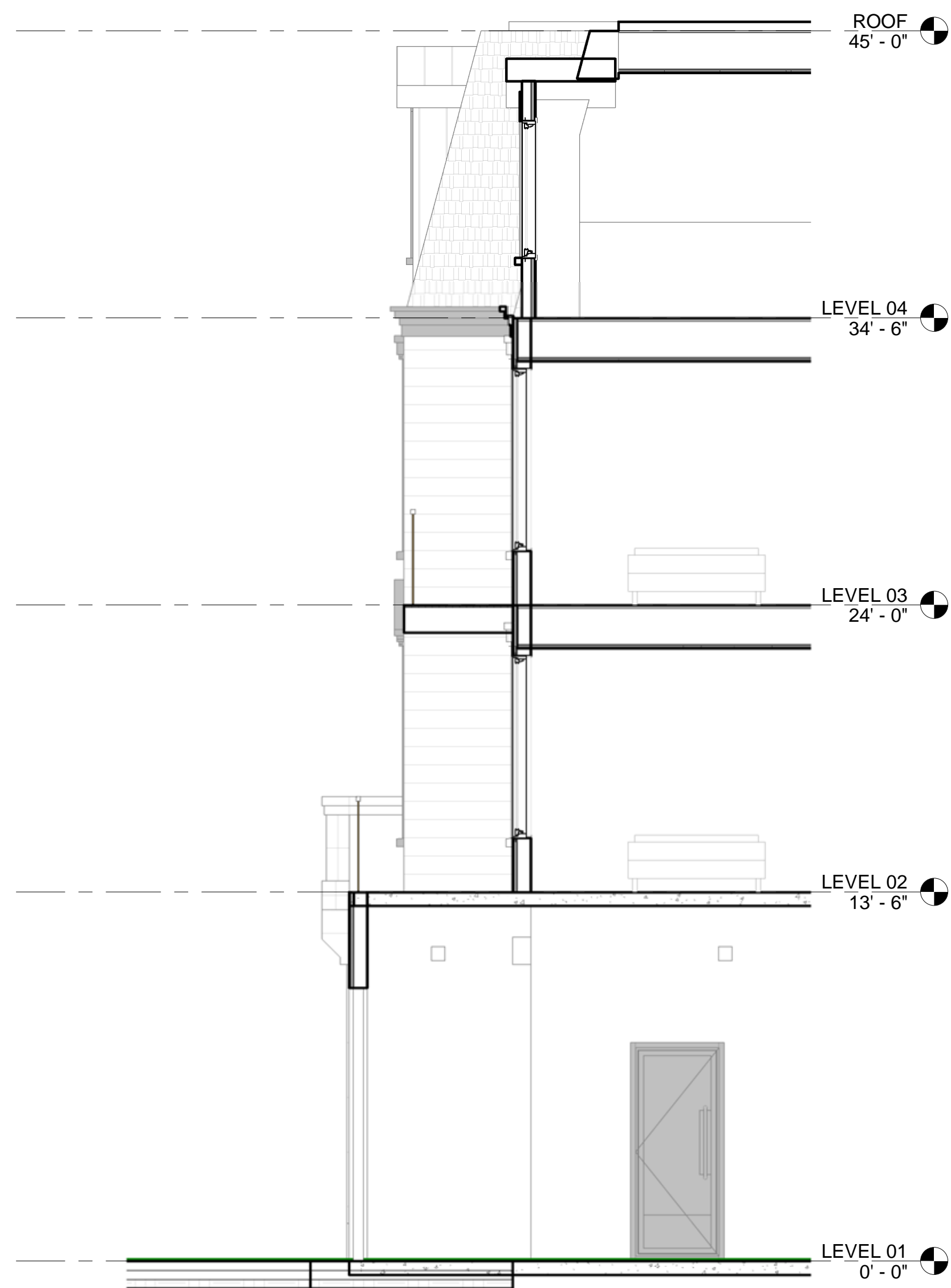
PROJECT NAME
**459 MAIN STREET
 REDEVELOPMENT**

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GCFodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492

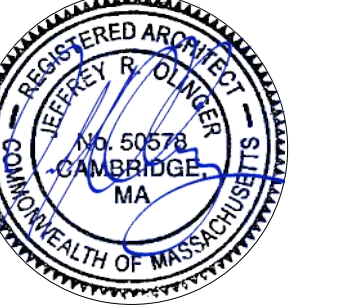


1 A411_GARAGE ENTRANCE WALL SECTION
 1/4" = 1'-0"

2 A411_SIDE YARD WALL SECTION 1
 1/4" = 1'-0"

3 A411_SIDE YARD WALL SECTION 2
 1/4" = 1'-0"

REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022
3	ZONING UPDATE	04/05/2022

WALL SECTIONS 2

A411

459 MAIN STREET

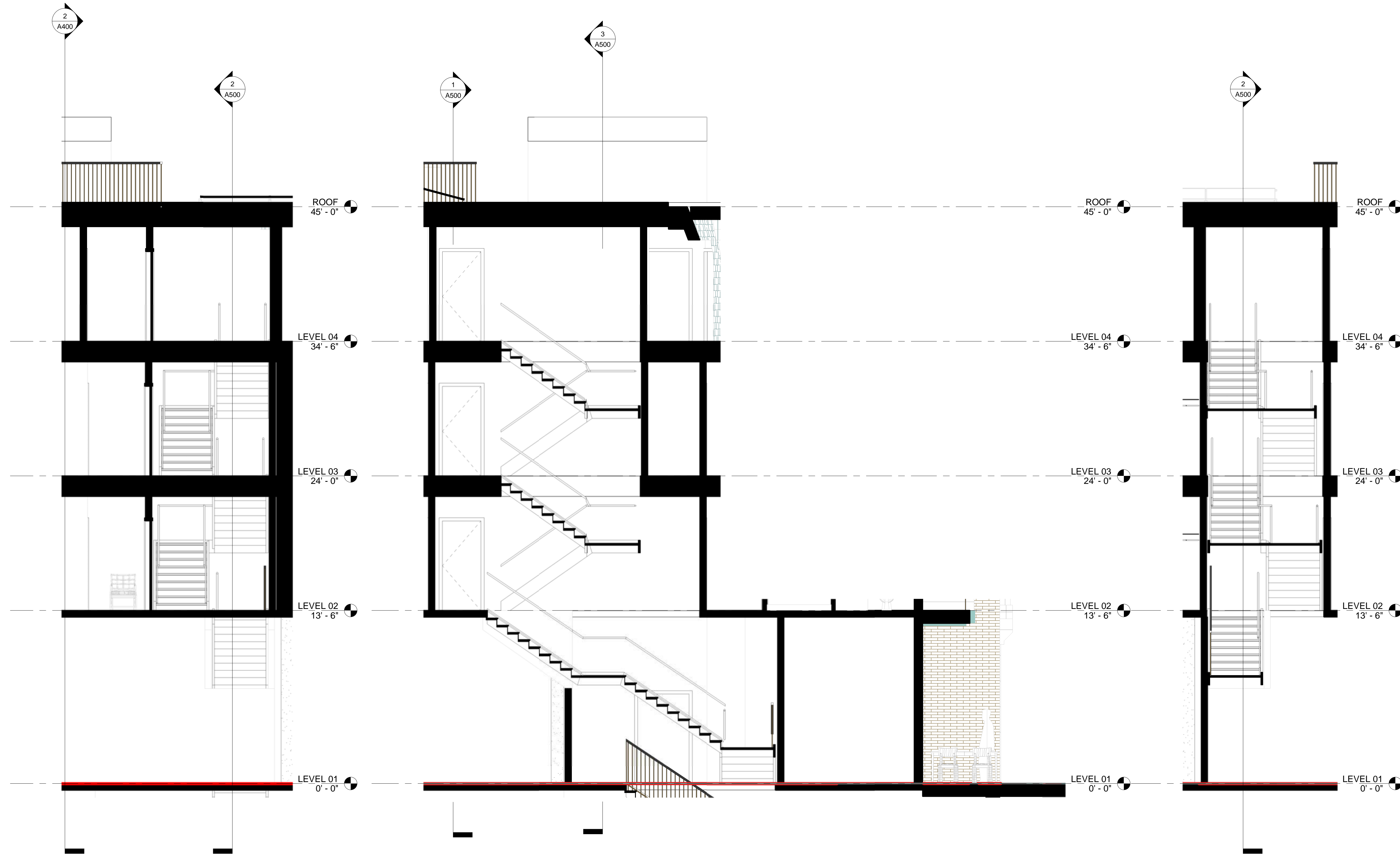
PROJECT NAME
459 MAIN STREET REDEVELOPMENT

PROJECT ADDRESS:
 459 MAIN STREET, READING, MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492

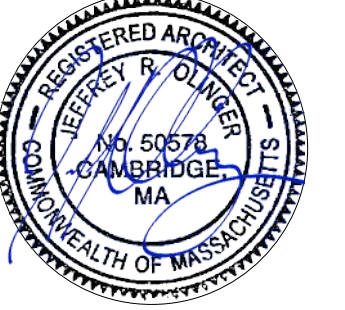


1 A500_STAIR 01 SECTION 03
 1/4" = 1'-0"

2 A500_STAIR 01 SECTION 01
 1/4" = 1'-0"

3 A500_STAIR 01 SECTION 02
 1/4" = 1'-0"

REGISTRATION



ZONING REVIEW

Project number 2122
 Date 01/18/2022
 Drawn by Author
 Checked by Checker
 Scale 1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

STAIR 01

A500

459 MAIN STREET

PROJECT NAME
459 MAIN STREET
REDEVELOPMENT

PROJECT ADDRESS:
 459 MAIN STREET, READING,
 MA 01867

APPLICANT
 Giuseppe C. Fodera
 GC Fodera Contracting Company
 28 Harbor Street
 Danvers, MA 01923

ARCHITECT
Olinger Architects
 77 PLEASANT STREET
 CAMBRIDGE, MA 02139
 (857) 777-6108
 INFO@OLINGER.IO

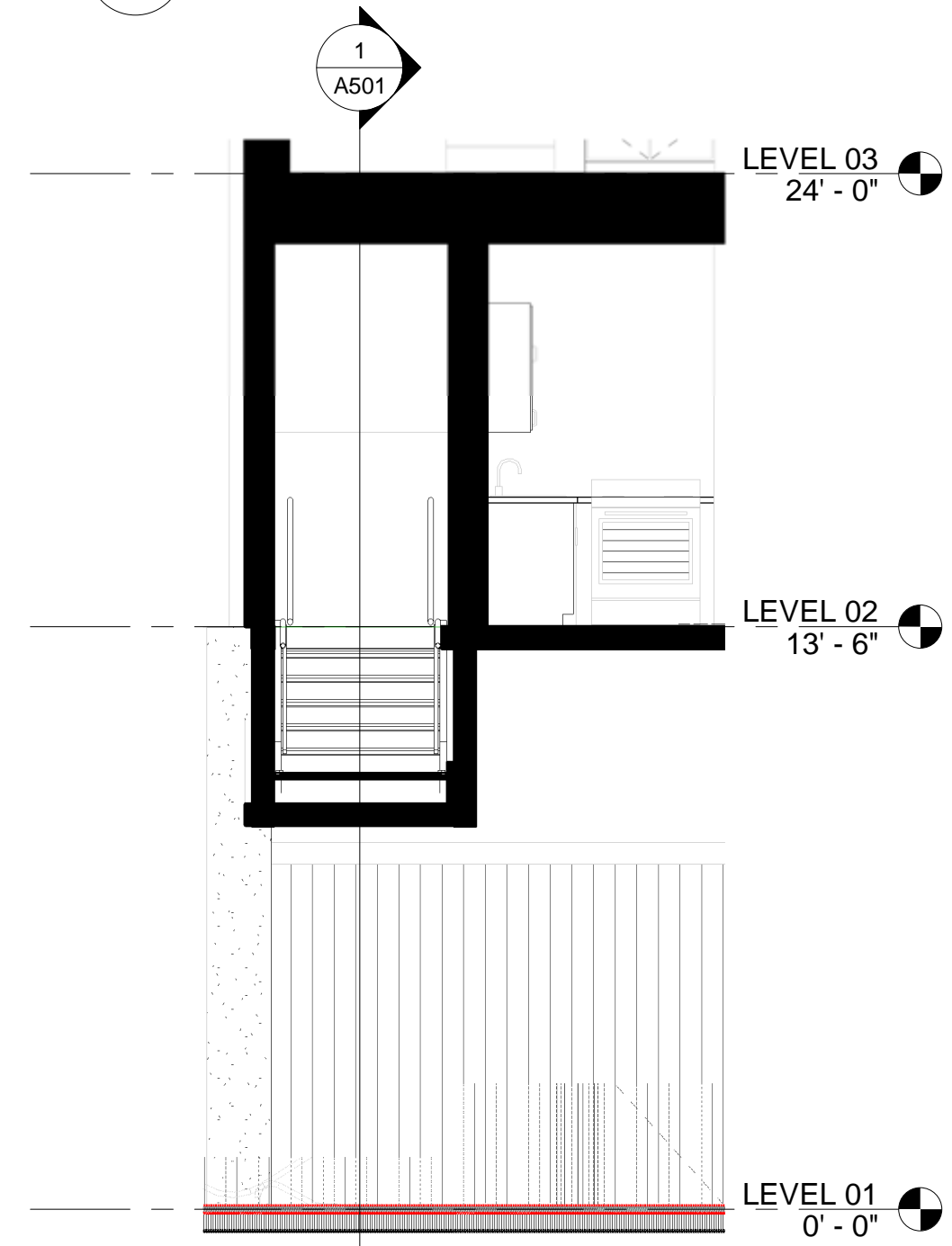
CIVIL ENGINEER
 FODERA
 ENGINEERING
 28 Harbor St., Suite 204
 Danvers, MA 01923
 Office: (617) 992-8492



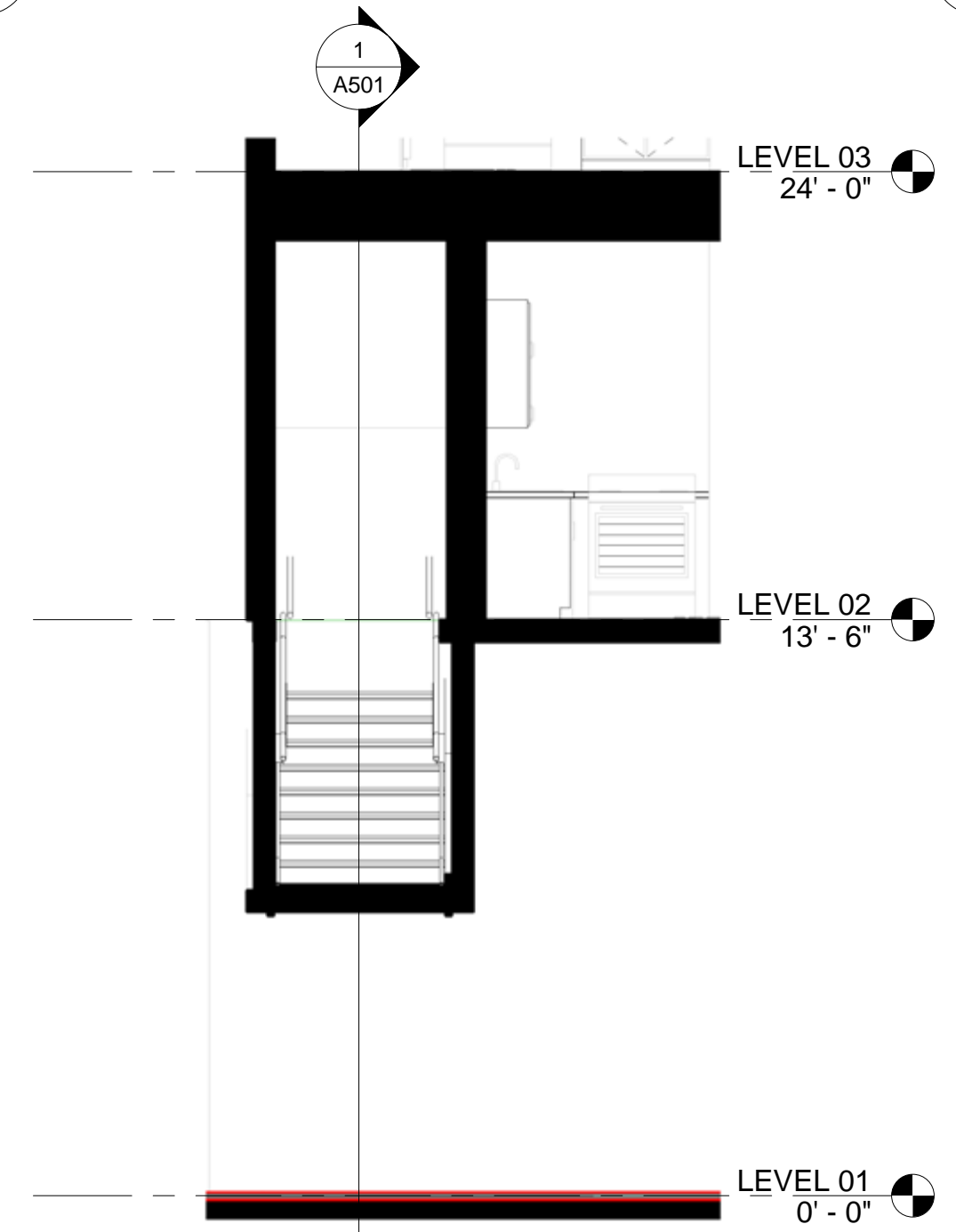
2 A500_STAIR 02 @ LANDING
 1/4" = 1'-0"

3 A500_STAIR 02 @ LEVEL
 1/4" = 1'-0"

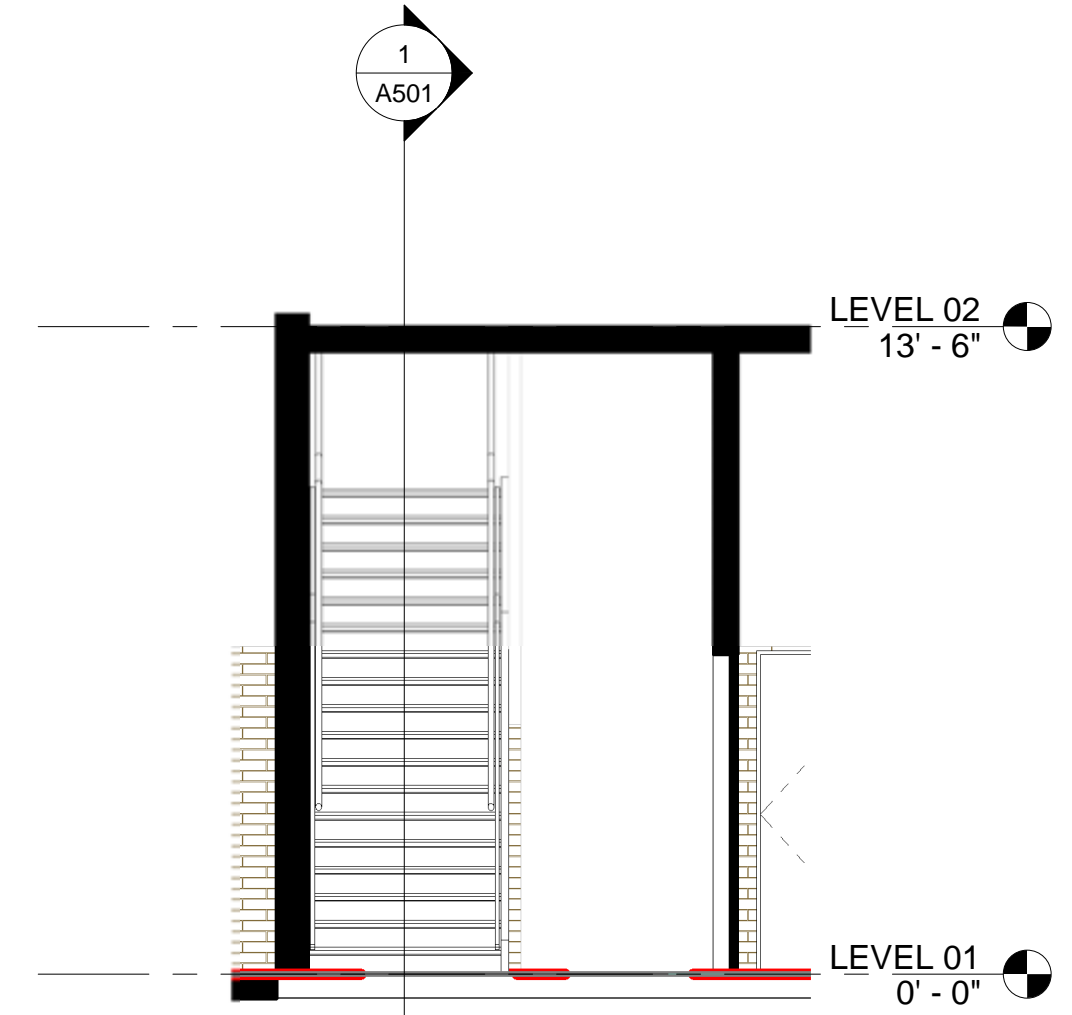
1 A502_STAIR 02 RUN SECTION
 1/4" = 1'-0"



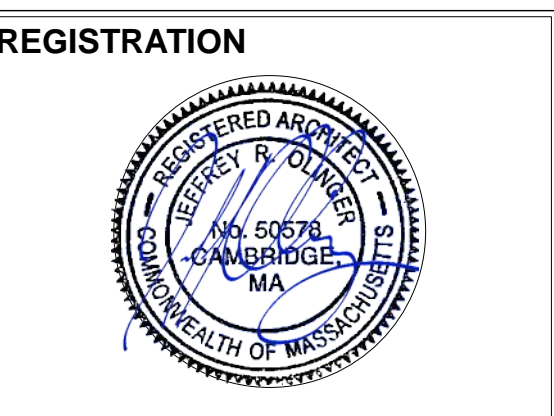
4 A500_STAIR 02 @ HUNG CORRIDOR
 1/4" = 1'-0"



5 A500_STAIR 02 @ HUNG CORRIDOR1
 1/4" = 1'-0"



6 A500_STAIR 02 @ HUNG CORRIDOR2
 1/4" = 1'-0"



ZONING REVIEW

Project number	2122
Date	01/18/2022
Drawn by	Author
Checked by	Checker
Scale	1/4" = 1'-0"

REVISIONS

No.	Description	Date
1	ZONING UPDATE	01/18/2022

STAIR 02

A501
 459 MAIN STREET

Trip Distribution Gravity Model - Residential

Project: T1184 - 459 Main Street - Reading, MA
 Date: 8/29/2022
 Analyst: TEC, Inc.
 Source: U.S. Census Bureau, 2019 MapIt Survey

Updated with 2019 Data*

Residence State -County-MCD Name	Workplace State -County-MCD Name	Count	% of Total Reading Workers	% of Distributed Workforce	Major Route Entering / Exiting					Major Route Entering / Exiting					
					MA-28 from north	MA-28 from south	Washington Street from east	Washington Street from west	Check	MA-28 from north	MA-28 from south	Washington Street from east	Washington Street from west	Check	
Reading Town Middlesex Co. MA	Boston City Suffolk Co. MA	3,024	21.95%	32.08%		100%				100%	0%	32%	0%	0%	32%
Reading Town Middlesex Co. MA	Reading Town Middlesex Co. MA	1004	7.29%	10.65%	67%			33%		100%	7%	0%	4%	0%	11%
Reading Town Middlesex Co. MA	Woburn City Middlesex Co. MA	668	4.85%	7.09%		67%			33%	100%	0%	5%	0%	2%	7%
Reading Town Middlesex Co. MA	Burlington Town Middlesex Co. MA	540	3.92%	5.73%		50%			50%	100%	0%	3%	0%	3%	6%
Reading Town Middlesex Co. MA	Cambridge City Middlesex Co. MA	652	4.73%	6.92%		100%				100%	0%	7%	0%	0%	7%
Reading Town Middlesex Co. MA	Stoneham Town Middlesex Co. MA	193	1.40%	2.05%		100%				100%	0%	2%	0%	0%	2%
Reading Town Middlesex Co. MA	Wilmington Town Middlesex Co. MA	308	2.24%	3.27%	67%				33%	100%	2%	0%	0%	1%	3%
Reading Town Middlesex Co. MA	Wakefield Town Middlesex Co. MA	367	2.66%	3.89%		67%		33%		100%	0%	3%	1%	0%	4%
Reading Town Middlesex Co. MA	Winchester Town Middlesex Co. MA	228	1.66%	2.42%		50%			50%	100%	0%	1%	0%	1%	2%
Reading Town Middlesex Co. MA	Lexington Town Middlesex Co. MA	178	1.29%	1.89%		100%				100%	0%	2%	0%	0%	2%
Reading Town Middlesex Co. MA	Medford City Middlesex Co. MA	214	1.55%	2.27%		50%			50%	100%	0%	1%	0%	1%	2%
Reading Town Middlesex Co. MA	Waltham City Middlesex Co. MA	352	2.56%	3.73%		100%				100%	0%	4%	0%	0%	4%
Reading Town Middlesex Co. MA	Andover Town Essex Co. MA	375	2.72%	3.98%	100%					100%	4%	0%	0%	0%	4%
Reading Town Middlesex Co. MA	Peabody City Essex Co. MA	199	1.44%	2.11%				100%		100%	0%	0%	2%	0%	2%
Reading Town Middlesex Co. MA	Billerica Town Middlesex Co. MA	177	1.29%	1.88%	50%	50%				100%	1%	1%	0%	0%	2%
Reading Town Middlesex Co. MA	Lynn City Essex County	172	1.25%	1.82%				100%		100%	0%	0%	2%	0%	2%
Reading Town Middlesex Co. MA	Somerville City Middlesex Co. MA	220	1.60%	2.33%		100%				100%	0%	2%	0%	0%	2%
Reading Town Middlesex Co. MA	North Reading Town Middlesex Co. MA	221	1.60%	2.34%	100%					100%	2%	0%	0%	0%	2%
Reading Town Middlesex Co. MA	Danvers Town Essex Co. MA	170	1.23%	1.80%				100%		100%	0%	0%	2%	0%	2%
Reading Town Middlesex Co. MA	Newton City Middlesex Co. MA	164	1.19%	1.74%		100%				100%	0%	2%	0%	0%	2%
TOTAL		13,774	68.43%	100.00%							16.5%	64.2%	10.6%	8.7%	100.0%
											Say	16%	64%	11%	9%



Jesse D. Schomer, Esq.
jschomer@regnante.com

Regnante Sterio LLP
Attorneys-at-Law
401 Edgewater Place, Suite 630
Wakefield, MA 01880
(781) 246-2525
regnante.com

September 7, 2022

Town of Reading
Community Planning and Development Commission
16 Lowell Street
Reading, MA 01867

Re: Application for DSGD Chapter 40R Plan Review: 459 Main Street
Applicant: GC Fodera Contracting, Inc.

Dear Commissioners:

This office is legal counsel to GC Fodera Contracting, Inc., the applicant with respect to the proposed Chapter 40R smart growth development located at 459 Main Street. Enclosed with this letter please find the following for filing in advance of the Commission's September 12 public hearing regarding this project:

- (1) Planning Update plan set entitled 459 Main Street Redevelopment (Revised September 7, 2022), containing updated architecture sheets prepared by Olinger Architects and updated civil engineering sheets prepared by Fodera Engineering.
- (2) Updated "Journey to Home" vehicle trip distribution chart dated August 29, 2022, prepared by TEC, Inc.

Additionally, I write to acknowledge the applicant's receipt of a letter dated August 18, 2022 the Commission's traffic peer review consultants (Green International Affiliates, Amy Allen, P.E., PTOE), as well as Town of Reading planning staff comments dated August 25, 2022 regarding this project. I have reproduced the salient comments and questions from these letters below, followed by the applicant's responses.

Comment: Please correct FAR calculation on page G002 Figure 5.B to 2.72 (includes required parking areas)

Response: This calculation of FAR was made in accordance with Section 2 of the Reading Zoning Bylaw and reflects the fact that the parking area is not enclosed by "exterior walls", and therefore is excluded under the definition of net floor area (as well as gross floor area, since it is non-habitable space). If, notwithstanding, it is preferred to interpret the parking area as constituting net floor area, the total would be 2.72. See sheet G002.

Comment: Please look to add an interior door to Trash Room for improved retail access.

Response: This change has been made as requested. See architecture sheet A101.

Comment: Please look to relocate interior retail door access to not be within proposed loading zone area for better access to such.

Response: This change has been made as requested. See architecture sheet A101.

Comment: Because some of the spaces in the parking garage are intended for the commercial land use, the Applicant should provide signage to indicate that commercial parking is available on-site.

Response: This change has been made as requested. See civil sheet C-2.

Comment: Please confirm Washington St garage door clearance/height and that delivery/emergency vehicles exit can be accommodated. Please confirm the lowest garage clearance. Does Stairwell #1 impact the 13'6" expected?

Response: The minimum clearance through the garage door, in the garage drive aisle, and in the loading zone is 10'. See architecture sheet A101. This height can accommodate standard delivery vans (height: 7'-0"), mail trucks (height: 7'-5"), ambulances (height: 7'-11"), delivery trucks (height: 9'-4"), and 20-foot moving trucks (height: 9'-9"). All of these vehicles can be accommodated in the loading zone and can navigate into, through, and out of the garage. See architecture sheets A002 & A410 and civil sheet C-6.

Please note: the 13'-6" measurements indicated on building section plans represent the height from the top of the first-floor plate to the top of the second-floor plate – not the interior clearance inside the garage, which will vary based on the second-floor structure, presence of utilities, etc. Stairwell #1 will reduce the clearance in this location but will not affect the use of this area of the garage. All garage clearances will comply with Building Code.

Comment: Please confirm that solar is to be installed from the onset. Or is the site only to be solar ready? If the latter when can solar install be anticipated?

Response: The building will be designed and built as solar ready, the decision whether/when to install solar will be determined by the condominium association unit owners.

Comment: Please consider patio/terrace doors that open inwards so as to not prevent utilization of already thin spaces. Sheets A102 and A103 show a balcony in the southwest corner that is accessed through bathroom and kitchen. This should be corrected and perhaps relocate balcony slightly to east so it is accessed through bedroom. Do all terrace/balconies maintain at least 4' in width?

Response: Outward opening doors have been shown due to superior building envelope sealing properties. However, the applicant will consider the feasibility of changing to

inwards opening doors. Full balcony details will be finalized and corrected in final construction documents (CDs).

Comment: Please provide note of No Left Turn signage to install within garage.

Response: This change has been made as requested. See civil sheet C-2.

Comment: Please remove 'Do Not Block' box proposed on Washington St on Sheet C-2 of Civil Plans.

Response: This change has been made as requested. See civil sheet C-2.

Comment: Please provide vehicle pavement markings for loading zone on Civil Sheets.

Response: This change has been made as requested. See civil sheet C-2.

Comment: Please revise trip distribution analysis for residential uses by utilizing the most recent census data

Response: TEC has revised the trip distribution based on the updated US Census Journey-to-Work information (enclosed). Although the residential trip distribution percentage per approach slightly changes, the overall number of trips per approach does not change with these distribution changes. This is based on the low number of overall residential trips generated by the proposed site. Note that this comment does not apply to retail related trips.

Comment: Please provide staff and GIA turning movements out of the garage and the template that was used for the ambulance. Please provide all vehicle templates used to staff and GIA.

Response: The requested turning movements and templates have been provided and are shown on updated plans. See architecture sheets A002 & A410 and civil sheet C-6.

Comment: Please include analysis of what a left turn out of the property would do to the operation of the Washington/Main intersection, specifically related to the timing of movements – can Green Int review this analysis as well?

Response: The Traffic Impact Assessment (TIA) as prepared by TEC on March 3, 2022 provides an analysis of the Main Street/Washington Street intersection under a Build condition with the left-turn out of the Washington Street Driveway. The traffic study showed that based on the trip distribution characteristics of the site, it would be expected that a negligible number of vehicles would exit the site and turn left onto Washington Street (0 reported during AM/PM peak commute hours). The Applicant has committed to restrict left-turns from the driveway which would require all turns out of the driveway to turn right into the intersection. The TIA also indicates that the effect of traffic at the intersection (7 total vehicles in the AM peak hour and 9 total vehicles in the PM peak hour) would be generally unnoticeable (1 new vehicle on average every 6 minutes). The Applicant has committed to complete any retiming of the intersection in accordance with Green International's recommendation of pre-occupancy and post-occupancy (if necessary).

Comment: What improvements, recommended by GIA, to intersection is Applicant willing to commit to?

Response: The applicant is agreeable to a condition of approval requiring the applicant to deposit with the Town offsite infrastructure improvement/project mitigation funds in the sum of \$25,000.00 prior to issuance of building permits for vertical construction. These funds can be earmarked by the Town Engineer for such improvements as are deemed necessary by his office at the appropriate time.

Respectfully,

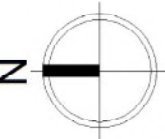
REGNANTE STERIO LLP

/s/ Jesse D. Schomer, Esq.
JESSE D. SCHOMER, ESQ.

Encls.

Town of Reading Oakland Road

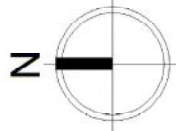
Planning Board Meeting
September 12, 2022



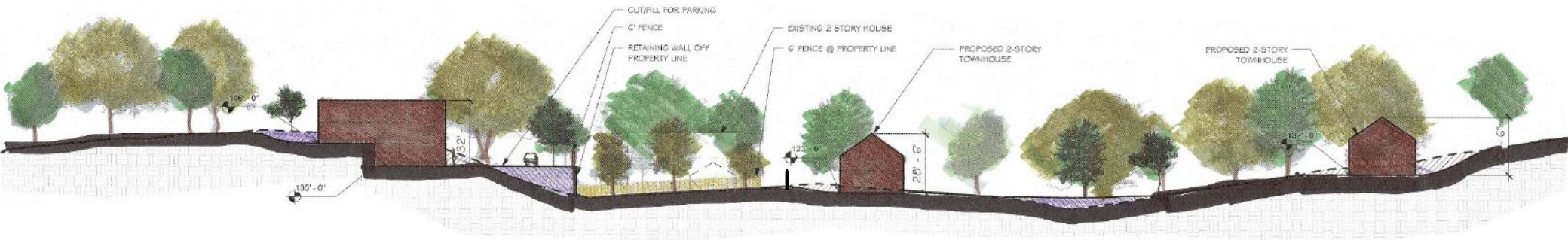


Total Site Area: 4.26 acres
 Total Dwelling Units: 47
 Units/Acre: 11.03

Site Plan

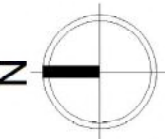


Site Cross Section



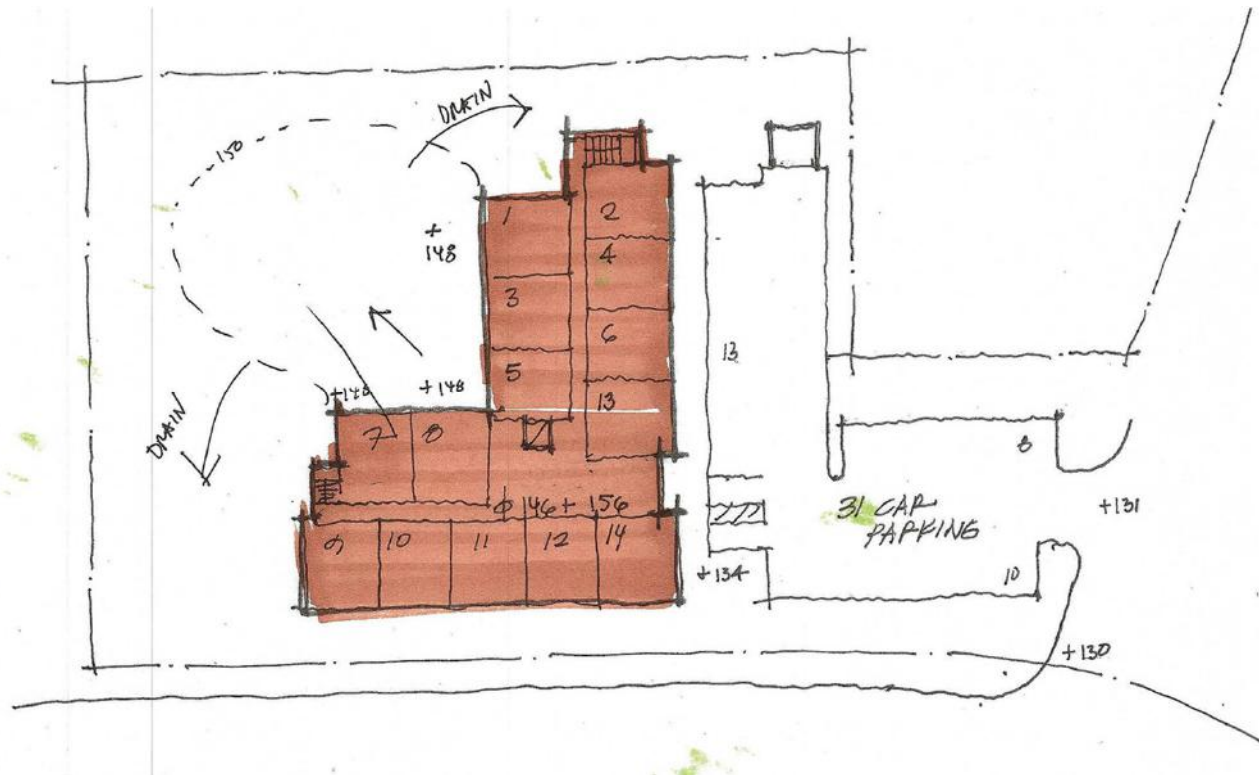


Family Housing Site Plan

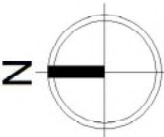


Elevations of Similar Family Housing





Senior Building Second and Third Floor Plan



Rendering of Similar Senior Development

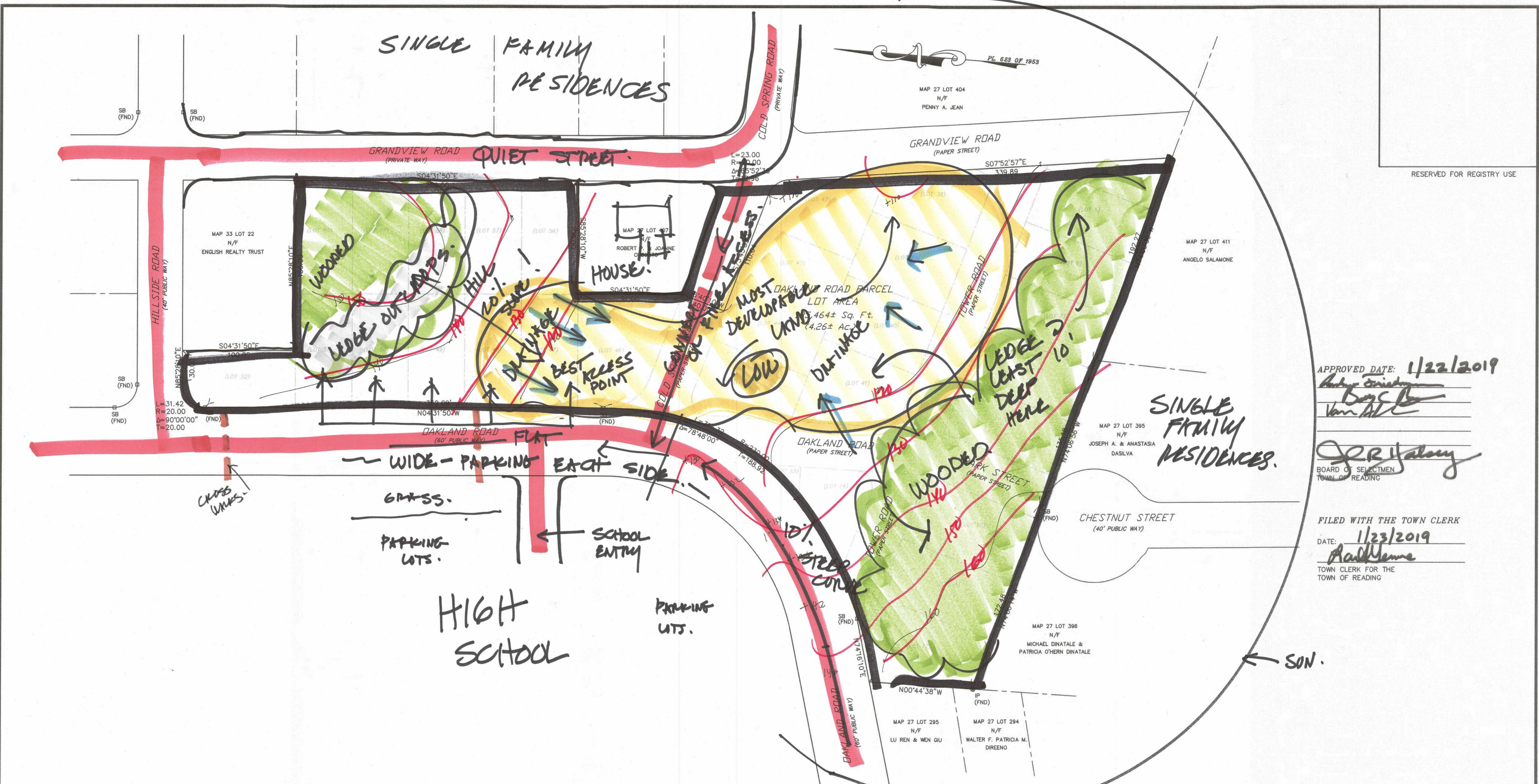


Drawing of Similar Senior Development



Thank You!





RESERVED FOR REGISTRY USE

APPROVED DATE: 1/22/2019
Robert C. ...
Boys ...
Van ...
J.R. ...
 BOARD OF SELECTMEN
 TOWN OF READING

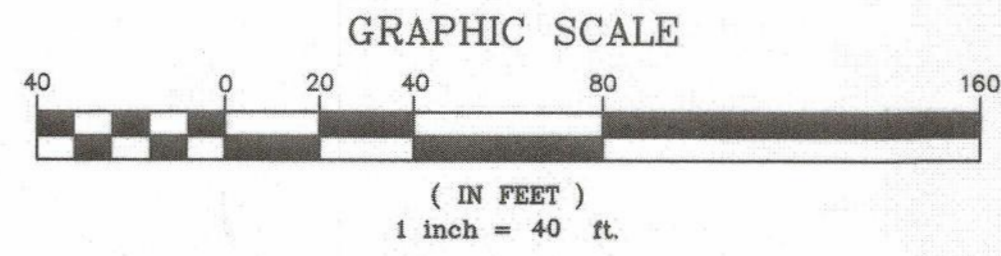
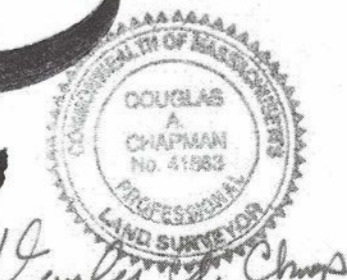
FILED WITH THE TOWN CLERK
 DATE: 1/23/2019
Paul ...
 TOWN CLERK FOR THE
 TOWN OF READING

SITE ANALYSIS

I CERTIFY THAT THIS PLAN IS PREPARED IN ACCORDANCE WITH THE RULES AND REGULATIONS OF THE MASSACHUSETTS REGISTRY OF DEEDS.

I CERTIFY THAT THE PROPERTY LINES SHOWN ARE THE LINES DIVIDING EXISTING OWNERSHIPS, AND THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED, AND THAT NO NEW LINES FOR DIVISION OF EXISTING OWNERSHIP OR FOR NEW WAYS ARE SHOWN.

Paul ... 9-22-16
 PROFESSIONAL LAND SURVEYOR DATE



- DEED REFERENCE:
- BK. 5791 PG. 523
 - BK. 6018 PG. 561
 - BK. 6089 PG. 32
 - BK. 6357 PG. 444
 - BK. 6472 PG. 329
 - BK. 7235 PG. 566
- PLAN REFERENCE:
- PLAN BK. 206 PG. 20
 - PLAN BK. 268 PG. 38
 - PLAN 623 OF 1953
 - PLAN 684 OF 1953
 - PLAN 2264 OF 1953
 - PLAN 1027 OF 1985
 - PLAN 954 OF 1988
 - PLAN 1299 OF 1989
 - PLAN 1180 OF 1996
 - PLAN 1251 OF 2000

NOTE:

PARCEL LOT NUMBERS REFER TO LOTS AS SHOWN ON PLAN BK. 206 PG. 20 AND PLAN BK. 268 PG. 38

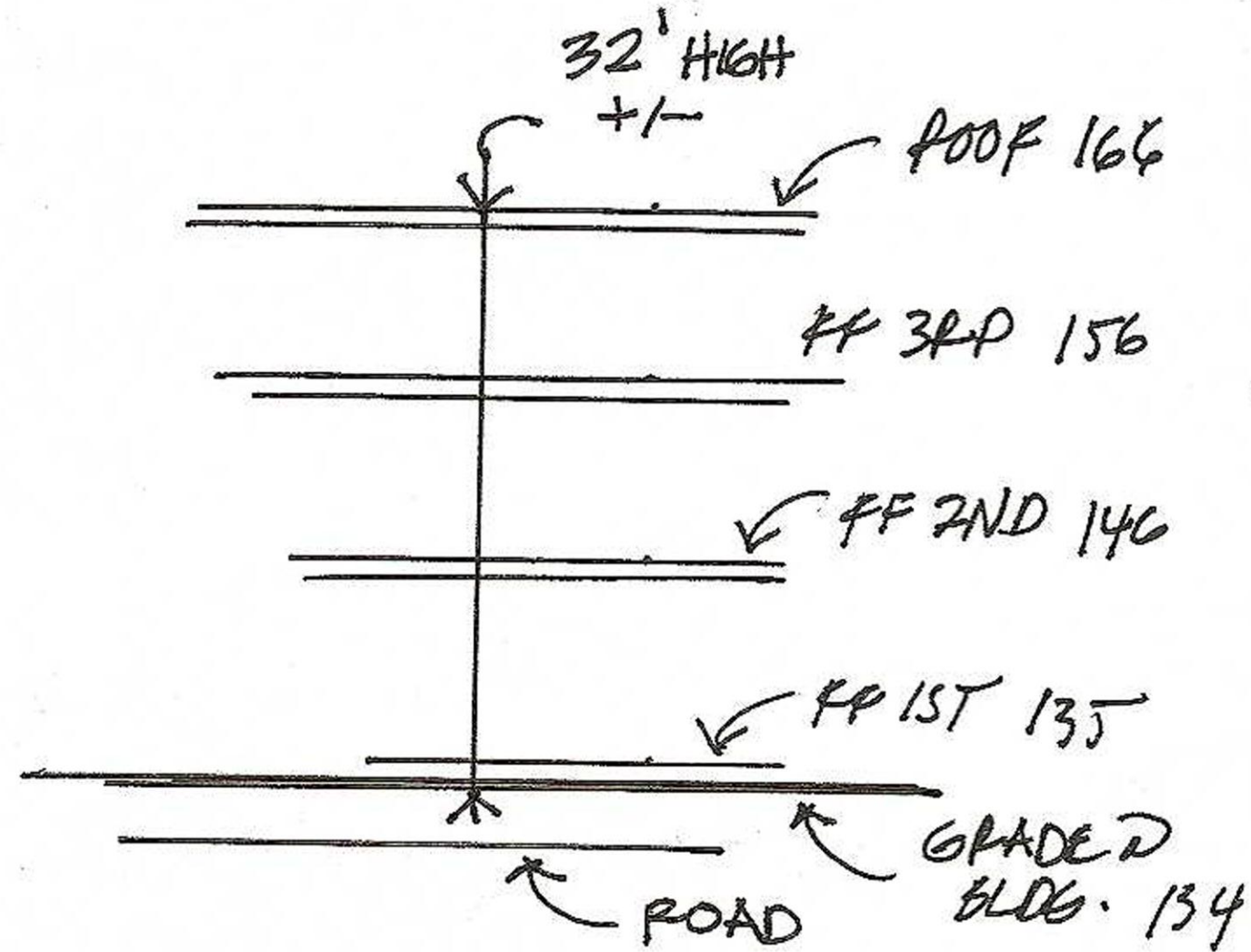
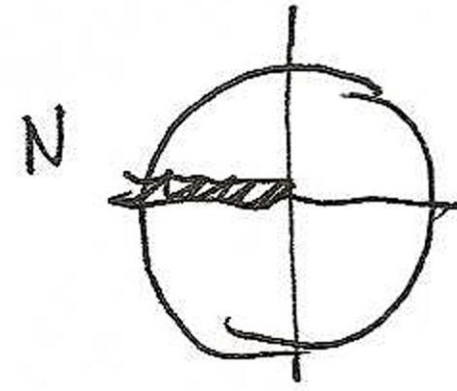
THIS PLAN IS BASED ON AN INSTRUMENT SURVEY PERFORMED ON OCTOBER 2011, APRIL 2013 AND NOVEMBER 2014, PLANS OF RECORD ON FILE AT THE MIDDLESEX SOUTH REGISTRY OF DEEDS AND READING ENGINEERING OFFICE.

PLAN OF LAND
 OAKLAND ROAD
 TOWN OF READING

SCALE: 1 IN = 40 FT DATE: SEP. 13, 2016

TOWN OF READING - DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION
 JEFFREY T. ZAGER, DIRECTOR D.P.W.
 RYAN PERCIVAL, P.E., TOWN ENGINEER

BAY STATE SURVEYING ASSOCIATES
 100 CUMMINGS CENTER
 BEVERLY, MA 01915

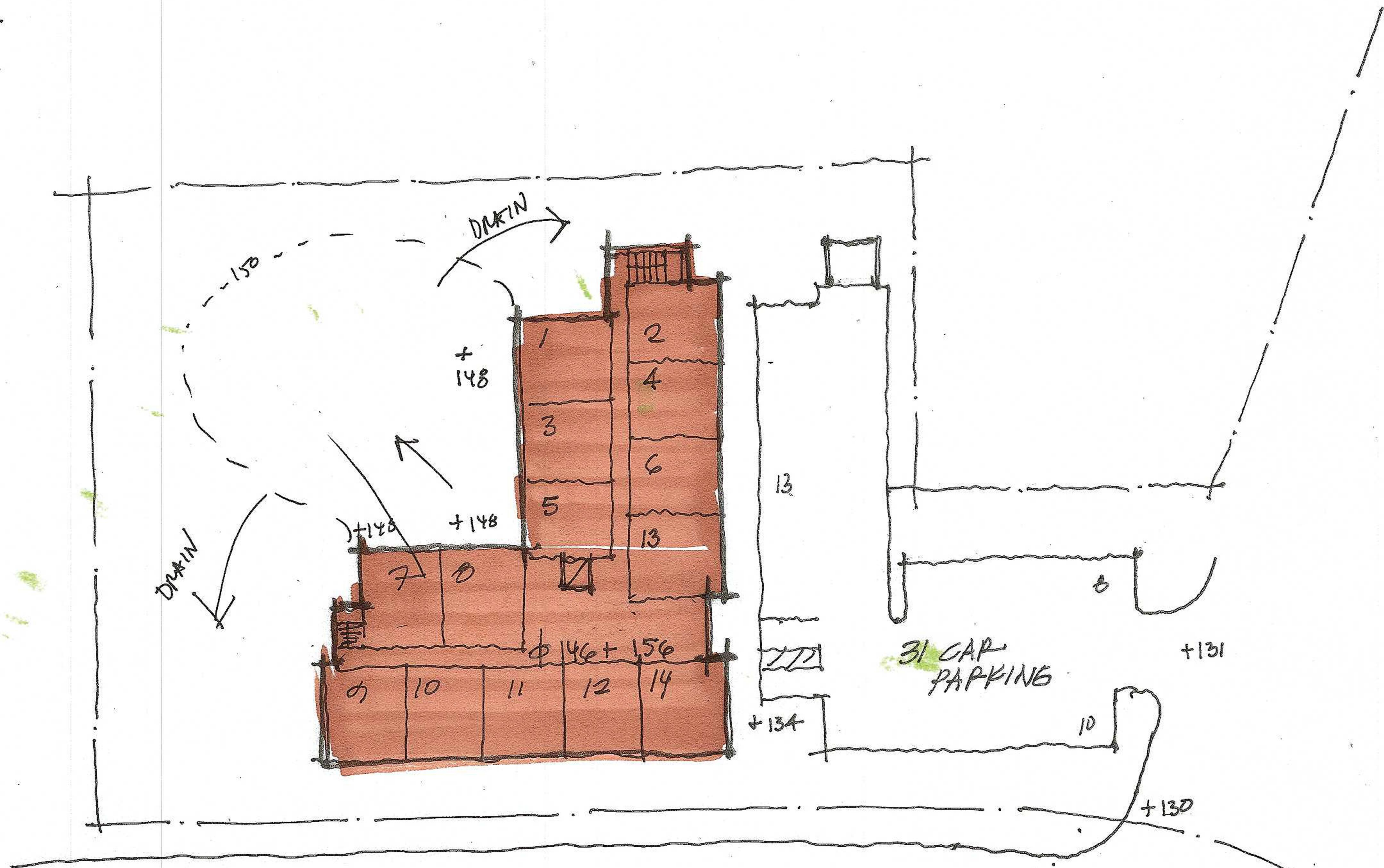
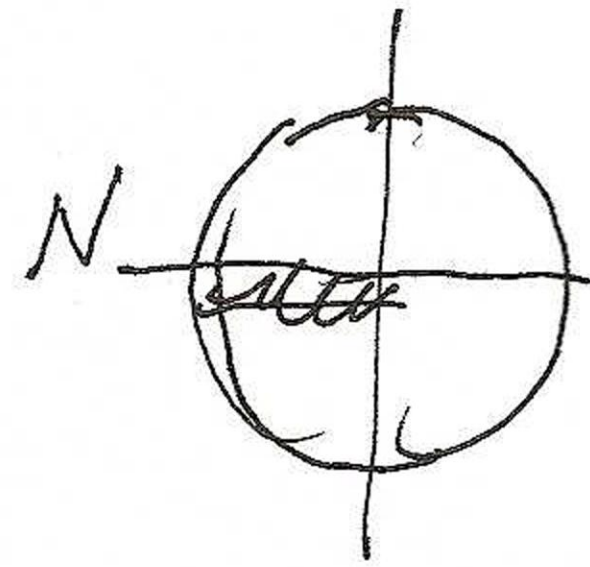


GROUND FLOOR / SITE PLAN.

SCALE 1"=20'-0"



17 JUNE 2022



FLOORS 2 & 3

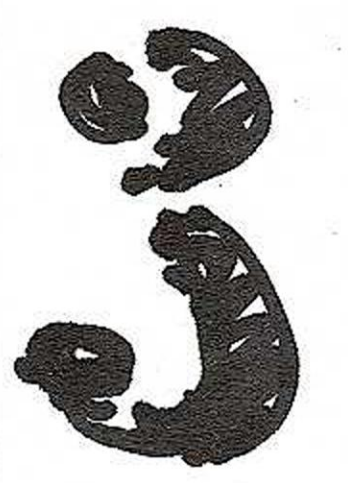
SCALE 1" = 20'-0"



17 JUNE 2022



16 UNITS
FAMILY
HOUSING



17 JUNE 2022



Town of Reading



**CPDC Presentation
September 12, 2022**



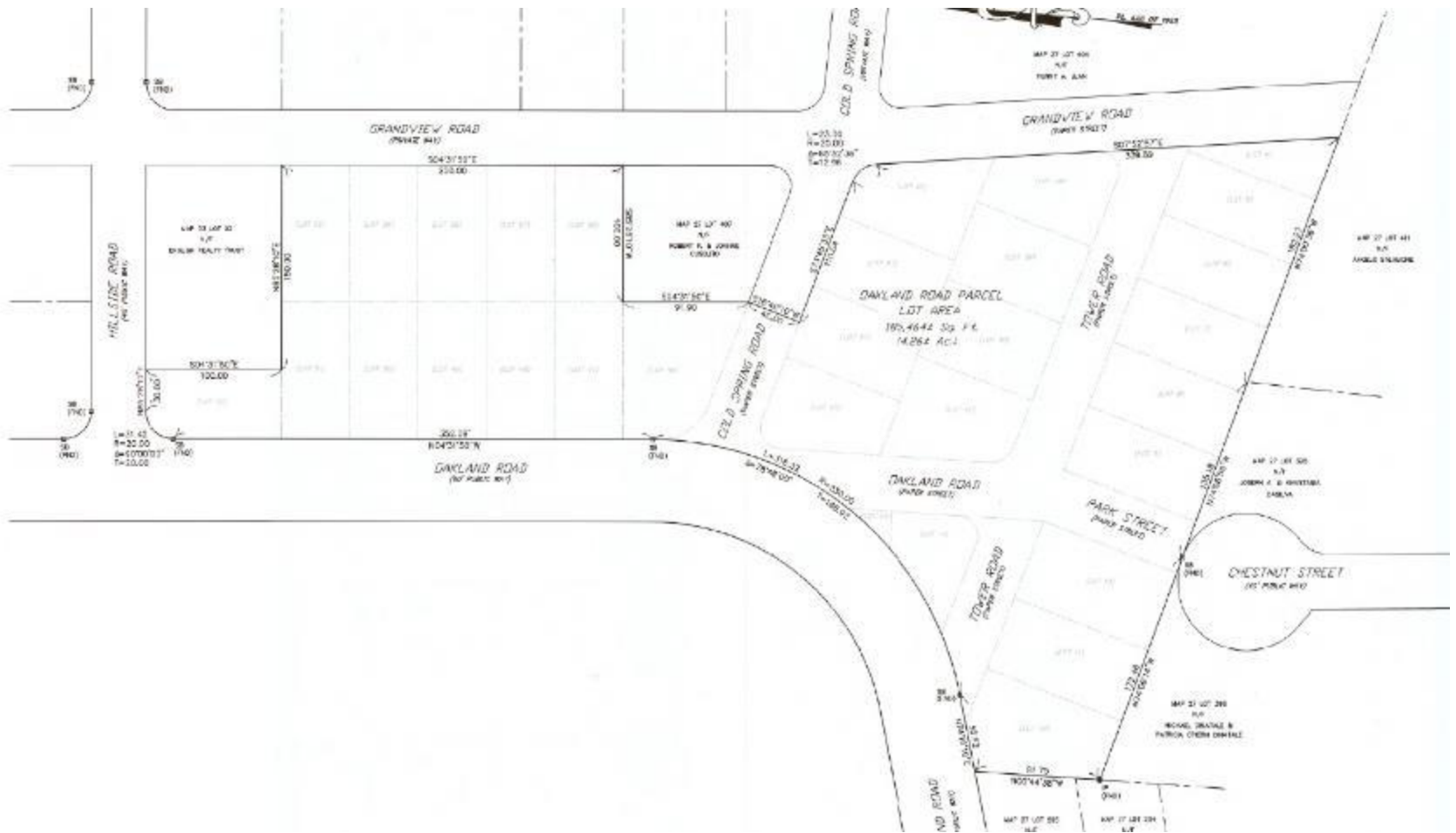


CPDC Oakland Road Project

- Introduction and Overview of Oakland Road Property – Fidel Maltez, Town of Reading
- Overview of Affordable Housing Needs in Reading, Katie Gallant, Reading Housing Authority
- Proposed Project for Oakland Road – Jen Hocherman, SV Design
- Questions & Answers



History of Oakland Road Property





Habit for Humanity Project - Weston

Weston Affordable Housing Trust Meeting
0 Wellesley St. Weston MA



2021



Habitat for Humanity Greater Boston has extensive **expertise and experience in building high quality attractive homes** that blend in with the neighborhood using both professional and volunteer labor. All prospective homeowners donate 250 to 300 hours of sweat equity in the construction of their homes. Habitat for Humanity Greater Boston provides the initial mortgages to the homeowners and assists in the management of the condominium associations to ensure that they are properly funded and maintained.


(617) 423 - 2223 ext.19


westonoutreach@habitatboston.org


www.habitatboston.org

• **Income:** Household income must be equal to or between 50%-80% of the area household median income for the Middlesex/Cambridge/Quincy HUD district *

Size of Household	50% of AMI	80% of AMI
1 person	\$49,100	\$78,300
2 person	\$56,100	\$89,500
3 person	\$63,100	\$100,700
4 person	\$70,100	\$111,850
5 person	\$75,750	\$120,800
6 person	\$81,350	\$129,750

*These numbers are current for 2022 and are subject to change April 2023.

• **First-Time Homebuyer**

• **Assets:** \$75,000 or less

• **Ability to Pay:**
Principal, interest, insurance, real estate taxes, and condo fee cannot be greater than 30% of gross monthly income

Allows for **local preference** for some of the six homes. The local preference includes families that meet the above requirements and are at least one of the following:

- Current Weston residents
- Current municipal employees or employees of Weston businesses or companies
- Families with children currently attending Weston schools, such as METCO students



Lynnfield Example – Colonial Village

1. Colonial Village was built by non-profit; no public funds used for the development or operation of the corporation or villages.
2. Occupants must be at least 58 years of age and come from the waiting list. Waiting list is comprised of current or former Lynnfield residents.
3. Colonial Village has a clubhouse located on site as an adjunct to the occupants unit. The clubhouse is used by residents without fee for sanctioned activities.
4. The “Village” is comprised of 12 townhomes clustered in groups of 2 or 3 units, each unit approximately 1,975 square feet in size. All units have their own private entrance and garage, allowing residents the privacy of being in a single family home, but all of the amenities within a condominium community.



